



Planning Commission Meeting Agenda

May 26, 2026
5:30 PM – Regular Meeting
Council Chambers
400 N. Douty St.

Planning Commissioners will meet in-person in the Council Chambers. The meeting will also be live streamed on the City's website: <http://livestream.hanford.city/>

ROLL CALL:

INVOCATION:

FLAG SALUTE:

PUBLIC COMMENT:

This is the time for citizens to comment on subject matters not on the agenda and that are within the jurisdiction of the Commission. This is also the public's opportunity to request an item from the Consent Calendar be pulled for discussion purposes or to comment on any item on the agenda. Comments related to Public Hearing items will be heard at the time the item is discussed. A maximum of three minutes is allowed for each speaker. Please begin your comments by stating your name and providing your city of residence.

CONSENT CALENDAR:

Consent Calendar items are considered routine and will be enacted in one motion. There will be no separate discussion of these matters unless a request is made and then the item will be removed from the Consent Calendar to be discussed and voted upon by a separate motion.

RECOGNITION:

- A. Community Development: Recognition of Gunther Norris for his service on the Planning Commission

PUBLIC HEARING:

- A. Community Development: Variance No. 0031-26: a request to deviate from the standards set forth in Section 17.38.060 of the Hanford Municipal Code, in order to locate bus parking spaces, solar canopy structures, electrical infrastructure, and allow for bus maneuverability within the required front setback area for the expansion of an existing Kings Area Regional Transit (KART) bus facility in the PF Public Facilities zone district. The project is located at 629 W Davis St. (APN 012-161-048).

DIRECTOR'S COMMENTS:

COMMISSIONERS' ITEMS OF INTEREST:

At this time, any Commissioner may ask a question for clarification of matters within the Planning Commission's jurisdiction, make an announcement, or report briefly on activities in their role as a Planning Commissioner. In addition, subject to the Planning Commissioner's Handbook, Commissioners may request staff to report back to the Planning Commission at a subsequent meeting concerning any matter within the Commissioner's jurisdiction (GC 54954.2).

ADJOURNMENT:

Materials related to an item on this Agenda submitted to the Planning Commission after distribution of the agenda packet are available to public inspection in the Planning Division Office located at 317 N. Douty Street, Hanford, California 93230, during normal business hours. Such documents are also available at the city's website, www.cityofhanfordca.com subject to staff's ability to post documents before the meeting. If you need a disability-related modification or accommodation, including auxiliary aids or services, to participate in this meeting, please contact the Planning Division's office, 559-585-2580, 317 N. Douty Street, Hanford, California 93230, at least 2 days prior to the meeting {28 CFR 35.102.35.104 ADA Title II} APPEALS: Any action of the Planning Commission, which is a final project decision, may be appealed to the City Council by the applicant or any adversely affected person. To file an appeal, an appeal application and filing fee of \$1,100.00 must be submitted to the Community Development Department, 317 N. Douty Street, Hanford, California, within ten (10) days following the date of the final decision of the Planning Commission.



AGENDA STAFF REPORT

MEETING DATE: 5/26/2026	AGENDA SECTION: PUBLIC HEARING
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SUBJECT:

Variance No. 0031-26: a request to deviate from the standards set forth in Section 17.38.060 of the Hanford Municipal Code, in order to locate bus parking spaces, solar canopy structures, electrical infrastructure, and allow for bus maneuverability within the required front setback area for the expansion of an existing Kings Area Regional Transit (KART) bus facility in the PF Public Facilities zone district. The project is located at 629 W Davis St. (APN 012-161-048).

STAFF RECOMMENDATION

Staff recommends that the Planning Commission:

1. Adopt Resolution No. 2026-08, approving Variance No. 0031-26.

RECOMMENDED MOTION

1. I move to adopt Resolution No. 2026-08, approving Variance No. 0031-26.

PROJECT DESCRIPTION

The project is a request for a variance to deviate from the standards set forth in Section 17.38.060 of the Hanford Municipal Code, in order to locate bus parking spaces, solar canopy structures, electrical infrastructure, and allow for bus maneuverability within the required front setback area for the expansion of an existing Kings Area Regional Transit (KART) bus facility in the PF Public Facilities zone district. The front building setback shall be 20 feet from the front lot line per Section 17.38.060. The applicant seeks to allow the solar canopy to encroach 17.6', and Battery Energy Storage Systems (BESS) to encroach 7.2' into the 20-foot front building set back area (see **Attachment 1**).

Entitlement

The expansion of the existing KART bus facility was approved under Site Plan Review No. 0170-26. All conditions set forth for Site Plan Review No. 0170-26, attached as **Attachment 2**, shall also be conditions of approval for this variance application, except where modified by this variance.

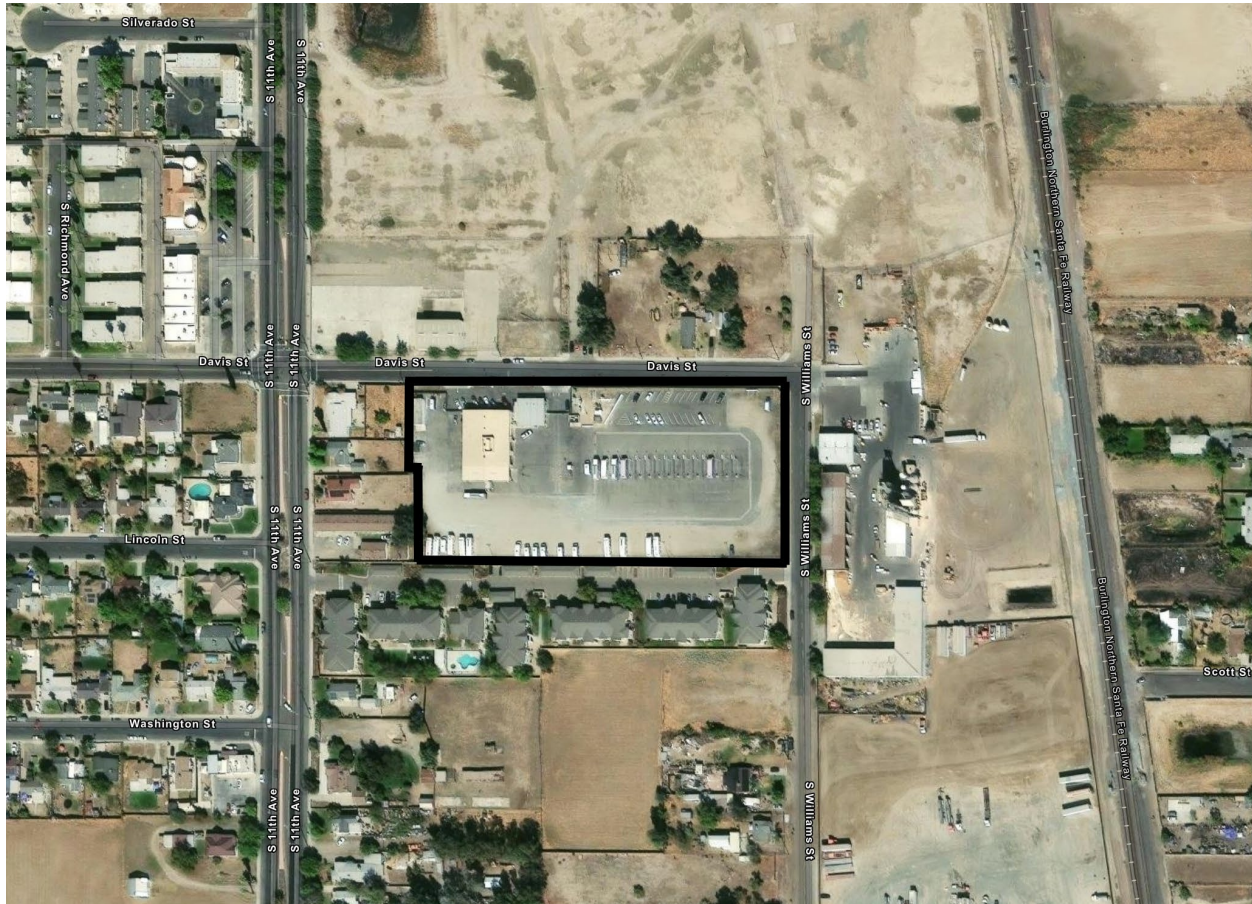
ATTACHMENTS:

1. VAR0031-26 Background
2. Attachment 1- Site Plan
3. Attachment 2- Approval of SPR0170-26
4. Attachment 3- Notice of Exemption No. 2026-30

BACKGROUND INFORMATION

The project site is located at 629 W. Davis St., the southwest corner of W. Davis St. and S. Williams St. (APN 012-161-048) in the City of Hanford. (see Figure 1, below)

Figure 1:
Land Use (property outlined in bold)



General Plan Designation

The General Plan designates the property as Public Facilities (see Figure 2, below). According to General Plan Policy L101, the Public Facilities land use designation accommodates municipal facilities and government operations. According to General Plan Policy L102, typical uses in the Public Facilities land use designation include public works yards, airports, waste water treatment plants, commuter parking lots, government offices, community centers, recreation centers, museums, fire stations, jails, and other similar activities conducted on property owned by the City, County or other State, federal, or local agencies. The expansion of the existing use of the site as a KART bus facility is consistent with the purpose and typical uses in the Public Facilities land use designation, as described in the General Plan. Further General Plan Policy L103, states to locate the Public Facilities land use designation on land owned by Hanford, Kings County, or other local or regional public agencies. This is an expansion of an existing use on a site owned by a local agency, the Kings County Area Public Transit Agency (KCAPTA).

Figure 2:
General Plan Designation (property outlined in bold)
Public Facilities



Zoning Designation

The property is zoned PF Public Facilities, which corresponds with the General Plan Designation (see Figure 3, below). The applicant proposes expanding the existing facility by constructing additional bus parking spaces, as well as installing solar canopies and electrical infrastructure. According to the Commercial, Office, and Industrial Zone Use Table, presented in Table 17.08.030 of the Hanford Municipal Code, a “public corporation yard, utility yard, or vehicle and heavy equipment maintenance and storage yard” is permitted by right in the PF Public Facilities zone district (see Figure 4, below). “public corporation yard, utility yard, or vehicle and heavy equipment maintenance and storage yard” means a facility that provides for maintenance, servicing, or storage of fleet vehicles, equipment, or supplies, or for the dispatching of service vehicles; or distribution of supplies or construction materials required in connection with a business activity, public utility service, transportation service, government service, or similar activity.

Figure 3:
Zoning Designation (property outlined in bold)
PF Public Facilities

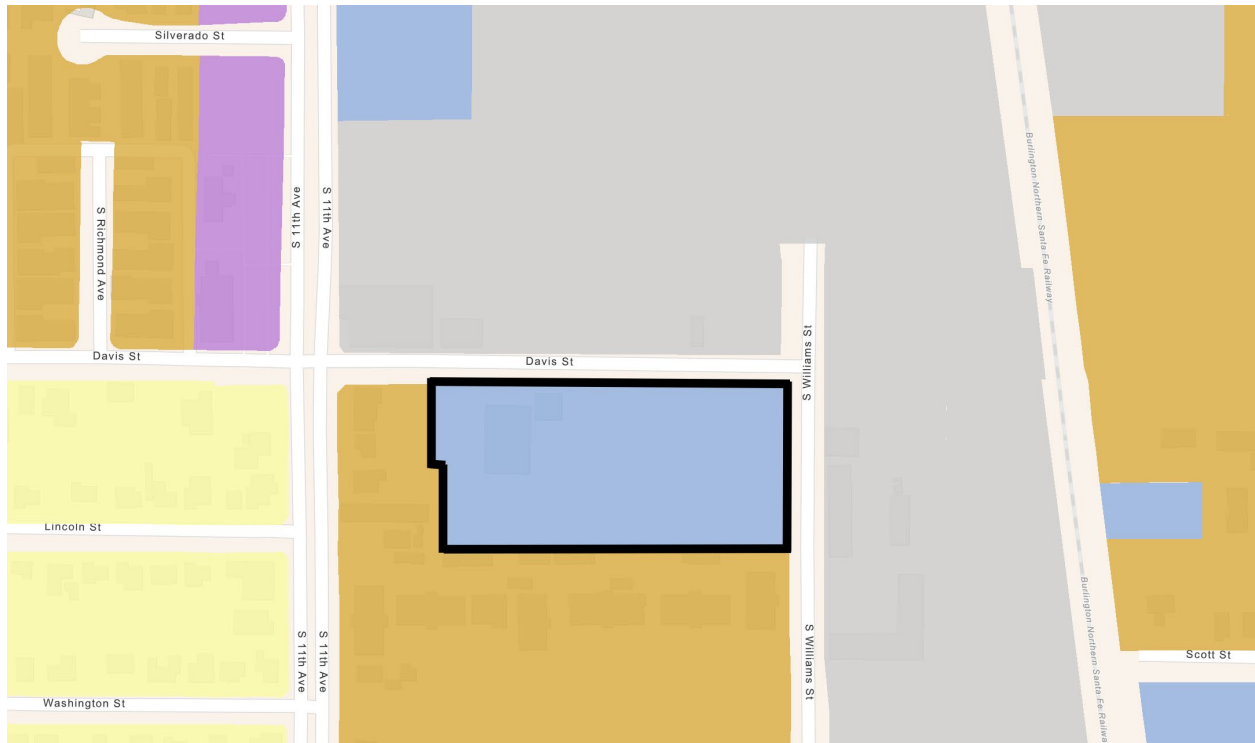


Figure 4:
Land Use Table

Commercial, Office, and Industrial Use Table			
P = Use is Permitted by Right C = Use Requires Conditional Use Permit A = Use Requires Administrative Use Permit T = Use Requires Temporary Use Permit Blank = Use is Not Allowed			
Land Uses		PF	Specific Land Use Standards (See identified Section)
F8	Fairgrounds	P	
F9	Government office; local, state, or federal	P	Federal and State agencies exempt from zoning
F10	Post office	P	
F11	Public corporation yard, utility yard, or vehicle and heavy equipment maintenance and storage yard	P	

PROJECT EVALUATION

Conformance with the standards of the PF Public Facilities zone district

17.38.030 Site area.

In the PF Public Facilities zone district, there is no minimum lot area.

17.38.040 Lot dimensions.

The minimum lot frontage shall be forty (40) feet.

The project site has a lot frontage of three hundred and five and a half feet (305.5') along Williams Street and approximately six hundred and forty five feet (645') along Davis Street which satisfies the forty (40) foot minimum lot frontage requirement.

17.38.050 Coverage.

The maximum coverage of a lot shall be determined by the combined building setback area requirements, accessory structure limitations, open space requirements, and off-street parking requirements.

All required building setback requirements, accessory structure limitations, open space requirements, and off-street parking requirements were evaluated and mandated as part of the site plan review process for the expansion of the existing bus facility. The site is in full compliance except for the front side building setback areas. This variance is sought to locate the bus parking spaces, solar canopy structures, and electrical infrastructure within the front building setback areas.

17.38.060 Building setback areas.

No structure shall be placed within a building setback area.

Front-Building Setback

In the PF Public Facilities zone district, the front building setback area shall be twenty (20) feet from the front lot line.

“Building setback area, front” means an area extending across the full width of the lot between the front lot line or the existing or future street right-of-way and a structural setback line parallel thereto. On corner lots, the shortest street frontage shall be the front building setback area.

The lot line along S. Williams St. is the front lot line, which requires a 20-foot front building setback area. The solar canopy and bus parking spaces are set back approximately 2.4 feet from the lot line along S. Williams St. The BESS is set back approximately 12.8' from the front lot line. For this reason, a variance has been submitted.

Rear-Building Setback

The rear building setback shall be fifteen (15) feet from the rear lot line, except where there is a landscape easement with a wall or fence on the rear of the lot then the rear building setback shall be twenty (20) feet.

“Rear” means a lot line, not intersecting a front lot line, which is most distant from and most closely parallel to the front lot line.

The proposed solar canopies and electrical infrastructure do not encroach into the rear setback areas. All proposed work is separated from the rear lot line by existing buildings to remain unchanged.

Side-Building Setback

The side building setback area shall be five (5) feet from an interior side lot line and ten (10) feet from a street side property line. Where there is a landscape easement with a wall or fence on the street side of the lot the side yard setback area shall be measured from the easement area instead of the side lot line.

The street-side lot line along W. Davis St. requires a 10-foot side-building setback area. The canopy posts are not encroaching into the 10-foot side-building setback along W. Davis St.

The interior side lot line requires a 5-foot setback area. The canopy posts are not encroaching into the 5-foot side-building setback area along the southern interior side lot line. Per Section 17.50.170(A), “architectural features, including without limitation, sills, chimneys, fireplaces, cornices and eaves, may extend into a required side building setback area, a required rear building setback area, or a space between structures, not more than 36 inches, and may extend into a required front building setback area not more than six feet; provided, that where an architectural feature extends more than 24 inches into a required side building setback area, such extension shall meet all fire and building code requirements.”

Justification for the encroachment is discussed further below.

17.38.070 Distances between structures.

The minimum distance between structures shall be ten (10) feet, except as provided by the building code.

The solar canopy along the front lot line is 5’ 7” from the solar canopy along the interior side lot line at their closest point. California Building Code allows for these Type U structures to be as close as 5 feet.

17.38.080 Height of structures.

The maximum structure height in the PF Public Facilities zone district is fifty (50) feet. The height of the canopies and all electrical infrastructure is less than fifty (50) feet.

17.38.090 Driveways.

Wherever possible developments shall share driveways to minimize the number of driveways on public streets. New driveways near street corners shall be located a minimum one hundred (100) feet from the radius curve of the curb, unless otherwise specifically approved by the City Engineer. Gated entrances shall be set back at least fifty (50) feet from the public right-of-way, unless a shorter distance is approved by the City Engineer.

There are no new driveways proposed for development. There are three existing driveways on the site. To access equipment on site, the driveway along Williams Street will be maintained, and shall be reconstructed to be consistent with City Standard CO-41 as required by the Engineering Division.

17.38.100 Accessory structures.

Accessory structures may be located within a required rear or side building setback area subject to all of the following:

The floor area shall be a maximum one hundred twenty (120) square feet. The walls shall be at least three (3) feet from rear and side lot lines. The roof eaves shall be at least two (2) feet from rear and side lot lines. The accessory structure shall be separated from other structures by a minimum of ten (10) feet. The plate line height shall be a maximum seven (7) feet high. The roof pitch shall be a maximum 4:12. The roof ridge peak height shall be a maximum of ten (10) feet.

“Accessory Building or Structure” means a structure which is subordinate to and the use of which is customarily incidental to, that of the main building, structure or use on the same site, including patio covers. If any accessory building is attached to the main building by a common wall or a connecting roof, such accessory building shall be deemed to be a part of the main building.

The proposed solar canopies and electrical equipment are accessory structures to the bus facility, and satisfy the development standards for accessory structures in the PF Public Facilities zone district.

17.38.110 Off-street parking.

Off-street parking and off-street loading facilities shall be provided on the site for each use as prescribed in Chapter 17.54. Required parking spaces may not be provided in within any front, side, or rear building setback area.

As per Table 17.54.040 of the Hanford Municipal Code, the land use most similar to the specified use of “public corporation yard, utility yard, or vehicle and heavy equipment maintenance and storage yard” would be “Post office; public corporation yard, utility yard, or vehicle and heavy equipment maintenance and storage yard; public safety facility or station”, which requires 1 space per employee of the maximum working shift. This project will maintain the existing 39 parking spaces for employees. Required parking spaces for employees do not encroach into the required setback areas. No additional employees will be added with construction of this project; therefore, no additional parking is required. The additional parking to be constructed is for bus storage only.

17.38.120 Usable open space.

In the PF Public Facilities zone district, there is no required usable open space.

17.38.130 Landscaping.

Landscaping shall be provided for each use as prescribed in Section 17.52 Landscape Standards. Except for driveways and approved parking areas, all yard areas and setback areas visible from a public street shall be landscaped with live plant materials and ornamentation common to the Hanford area or up to one hundred percent (100%) of the yard and setback area may be

landscaped with artificial turf and other permeable surfaces. Existing landscaping shall be maintained for the site.

17.38.140 Screening, fences, and walls.

Fences and walls shall be provided on the site for each use in accordance with Section 17.50.110 and 17.50.120. A block wall with a minimum height of seven (7) feet shall be provided along any side or rear lot line that abuts an R-L, R-M, R-H, OR, AP, or CO zone district. No fence or wall shall be placed in front of or within any landscaped area located next to a street. The open storage of materials and equipment attendant to a use shall be permitted only within an area surrounded or screened by a solid wall or fence seven (7) feet minimum in height. Such storage shall not be visible above the fence or wall.

There is an existing seven (7) foot CMU block wall along the interior-side lot line abutting the R-M Medium Density Residential zone district. This variance does not seek to alter the existing CMU block wall. Existing fencing along the front lot line and street-side lot line will remain and shall be repaired if damaged during construction. A new utility access gate is proposed at the southern end of the front lot line for equipment access.

17.38.150 Signs.

Signs placed in the PF zones shall be subject to the requirements and standards prescribed in Chapter 17.56, except that signs placed by a local, State, or Federal public agency on their own public property shall be exempt from Chapter 17.56.

There are no signs proposed with this project.

17.38.160 General provisions and standards.

Structures and the use of structures or land shall be subject to the requirements and standards prescribed in Chapter 17.50.

All requirements of the PF Public Facility zone district, prescribed by Chapter 17.38 of the Hanford Municipal Code, as well as the requirements of Development Standards, prescribed by Chapter 17.50 of the Hanford Municipal Code, are satisfied, except for encroachment into the front setback area, for which a variance is sought.

Variance Evaluation

In accordance with Section 17.38.060 of the Hanford Municipal Code, the front building setback shall be 20 feet from the front lot line. The applicant seeks to allow the solar canopy to encroach 17.6', and Battery Energy Storage Systems (BESS) to encroach 7.2' into the 20-foot front building set back area. Staff is able to support the variance on the basis of the unique, challenging lot configuration created by the requirements of the proposed land use type on a lot under 5 acres.

Lot Configuration

The property is a corner lot under 5 acres, surrounded on two sides by residential development, and abutted by streets on the north and east sides. The parcel has a unique and challenging lot size to accommodate the required infrastructure improvements, to support the expansion of the current use.

Other properties within the PF Public Facilities zone district do not possess this challenging, small lot size, nor are other uses in the PF Public Facilities located on a stand-alone parcel, not adjacent to other PF Public Facilities zoned properties. The lot size and inability to expand the property due to the existing development on all four sides create a special circumstance to allow for a variance to accommodate all of the necessary storage, equipment, and maneuverability of buses.

The General Plan encourages adequate and accessible service use development.

Goal L28: Adequate and accessible civic, utility, institutional, cultural, and service uses supporting the needs of Hanford's residents and businesses.

Goal L29: Appropriate sites for public facilities to serve projected population growth.

Policy L101 Purpose of Public Facilities Land Use Designation

Establish the Public Facilities land use designation to accommodate municipal facilities and government operations.

Policy L102 Types of Uses in Public Facilities Land Use Designation

Typical uses in the Public Facilities land use designation include public works yards, airports, waste water treatment plants, commuter parking lots, government offices, community centers, recreation centers, museums, fire stations, jails, and other similar activities conducted on property owned by the City, County or other State, federal, or local agencies.

Policy L103 Location of Public Facilities Designation

Locate the Public Facilities land use designation on land owned by Hanford, Kings County, or other local or regional public agencies.

Allowance of the variance will facilitate the expansion of the existing use on the site, without compromising bus maneuverability. Due to ICT regulations, KCAPTA must deploy the infrastructure necessary to support electric bus operations. This infrastructure includes solar canopy charging structures, electric bus charging equipment, Battery Energy Storage Systems (BESS), and electrical switchgear and associated infrastructure. The applicant modified the site plan to locate all supporting infrastructure, maintain existing buildings to remain in operation, required employee parking, the openness of the central yard to accommodate buses' large turning radii and maneuvering space, and locate all BESS equipment at least twenty (20) feet from compressed natural gas (CNG) storage per California Fire Code (CFC) Chapter 23. The site plan as proposed to accommodate these requirements resulted in the minor encroachment into the 20-foot front building setback area. The variance will allow for the minor encroachment into the setback. The variance will not allow development to locate within the right-of-way.

FINDINGS FOR APPROVAL

Pursuant to Section 17.84.050 of the Hanford Municipal Code, seven findings must be made before approval of the variance application.

1. There are special circumstances applicable to the property, including size, shape, topography, location or surroundings, are such that the strict application deprives such property of privileges enjoyed by other property in the vicinity that are in the same zone district;

Analysis: That the special circumstance applicable to the property is the lot configuration.

The size of this lot does not adequately provide the space needed to allow for bus parking for the entire fleet, electrical infrastructure needed to support EV bus charging, employee parking, solar canopies, and allow for bus maneuverability without minor encroachment into the front setback area. The property is less than five acres in size.

2. The variance is necessary for the preservation and enjoyment of a substantial property right possessed by other property in the vicinity and that are in the same zone district and denied to the property for which the variance is sought;

Analysis: That the variance is necessary for the preservation and enjoyment of a substantial property right possessed by other properties in the vicinity in the same zone district. Due to the typical lot size of other similar properties in the PF Public Facilities zone district, other properties in the same zone district have the ability for growth without needing to encroach into required setback areas. Additionally, due to requirements from the State of California, other properties have the right to develop without consideration of standards to accommodate electric bus operations.

3. The variance will not be materially detrimental to the public health, safety or welfare, or injurious to the property or improvements in the vicinity and that are in the same zone district in which the property is located;

Analysis: That allowing encroachment into the 20-foot front set back area will not create a safety hazard. This proposed site plan has been reviewed by the various City departments and evaluated for conformity with the requirements of the Hanford Municipal Code Section 17.38 for the Public Facilities zone district. The use is consistent with the regulations set forth in the Hanford Municipal Code Section 17.38, with the exception of encroachment into the front setback area. Any other improvements or mitigations required for public health, safety, and welfare have been applied to the project as conditions of approval in the resolution. The variance will not allow development to locate within the right-of-way.

4. The variance does not constitute a special privilege inconsistent with the limitations upon other properties in the vicinity and that are in the same land use district in which such property is located;

Analysis: That granting the variance does not constitute a special privilege inconsistent with the limitations upon other properties in the vicinity that are in the same zone district, since other properties within the vicinity do not possess the same lot configuration or State regulation requirements.

The current fleet size stored at this yard is 39 buses and is expected to grow to 50 buses by 2040. As adopted by CARB in December 2018 and effective October 2019, the ICT regulation requires all California public transit agencies to gradually transition their bus fleets to 100% zero-emission buses (ZEBs) by 2040. To comply with this regulation, KCAPTA must deploy the infrastructure necessary to support electric bus operations. The lot configuration creates

a special circumstance which requires encroachment into the front setback area to accommodate all of the necessary storage, equipment, and maneuverability of buses.

5. The variance does not allow a use or activity which is prohibited in the zone district where the property is located;

Analysis: That the variance applies to a setback reduction for an allowable use in the PF Public Facilities zone district. The site is proposed to be an expansion of an existing bus facility, which is permitted in the PF Public Facilities zone district.

6. The variance is consistent with the purposes of this title; and

Analysis: That the variance is consistent with the purposes of this title to allow variances when, because of special circumstances applicable to the property, including location, shape, size, surroundings, topography, or other physical conditions, the strict application of the standards denies the property owner privileges enjoyed by other property owners in the vicinity and in the same zoning district.

The special circumstance applicable to the property is the lot configuration, which restricts alternative design options. In addition, maintaining full operations of existing buildings, bus maneuverability in the central yard, and separation between the BESS equipment and CNG storage all prevent alternative design options.

The current fleet size stored at this yard is 39 buses and is expected to grow to 50 buses by 2040. The lot configuration creates a special circumstance to allow for a variance to accommodate all of the necessary storage, equipment, and maneuverability of buses.

7. The variance will be consistent with the General Plan.

Analysis: According to General Plan Policy L101, the Public Facilities land use designation accommodates municipal facilities and government operations. According to General Plan Policy L102, typical uses in the Public Facilities land use designation include public works yards, airports, waste water treatment plants, commuter parking lots, government offices, community centers, recreation centers, museums, fire stations, jails, and other similar activities conducted on property owned by the City, County or other State, federal, or local agencies. The expansion of the existing use of the site as a KART bus facility is consistent with the purpose and typical uses in the Public Facilities land use designation, as described in the General Plan. Further General Plan Policy L103, states to locate the Public Facilities land use designation on land owned by Hanford, Kings County, or other local or regional public agencies. This is an expansion of an existing use on a site owned by a local agency, the Kings County Area Public Transit Agency (KCAPTA). Additionally, the project is consistent with the Public Facilities Land Use Designation Goals L28 and L29.

Goal L28: Adequate and accessible civic, utility, institutional, cultural, and service uses supporting the needs of Hanford's residents and businesses.

Goal L29: Appropriate sites for public facilities to serve projected population growth.

ENVIRONMENTAL ASSESSMENT

This project is categorically exempt from further environmental review, as per Section 15303 New Construction or Conversion of Small Structures of the California Environmental Quality Act (CEQA) Guidelines. There are no exceptions to this exemption, as per Section 15300.2 Exceptions of the California Environmental Quality Act (CEQA) Guidelines. A Notice of Exemption has been prepared and is attached as **Attachment 3**.

PUBLIC COMMENTS

Noticing of the variance and addendum was published in the newspaper on May 13, 2026 and mailed to property owners within 300 feet of the project site on May 14, 2026. The Community Development Department has not received comments on the project as of the preparation of this report.

CONDITIONS OF APPROVAL

Project approval shall be subject to the condition below:

Variance No. 0031-26:

1. All conditions set forth for Site Plan Review No. SPR0170-26 shall be conditions of approval for this variance, except where modified by this variance.

STAFF RECOMMENDATION

Staff recommends that the Planning Commission:

1. Adopt Resolution No. 2026-08, approving Variance No. 0031-26.

Applicant:

Alexander Dubney
401 B St. Suite 600
San Diego, CA 92101

Property Owner:

Kings County Area Public Transit Agency
1400 W. Lacey Blvd
Hanford, CA 93230



KINGS AREA REGIONAL TRANSIT (KART)
ELECTRIC BUS CHARGING PROJECT

VARIANCE APPLICATION

04/09/2026

Site Plan Review Number: SPR0170-26
629 W. Davis Street
Hanford, CA 93230

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JUSTIFICATION

1. Project Description

The Kings County Area Public Transit Agency (KCAPTA) operates the primary public transit system serving Kings County and the City of Hanford. KCAPTA currently operates 15 fixed routes, on-demand services, and vanpool programs serving approximately 715,000 riders annually, many of whom are predominantly low-income residents who rely on public transit for essential mobility.

The project site contains KCAPTA's existing transit maintenance facility, which provides maintenance and storage for the agency's fleet. The facility currently accommodates 39 buses, with projected growth to approximately 50 buses by 2040.

In accordance with the California Air Resources Board (CARB) Innovative Clean Transit (ICT) regulation, public transit agencies are required to transition to zero-emission bus fleets beginning in 2026, after which combustion-engine buses may no longer be purchased.

To comply with this requirement, KCAPTA proposes to install infrastructure necessary to support electric bus (EV) charging at the existing maintenance yard. The project includes:

- Solar canopy charging structures
- Electric bus charging equipment
- Battery Energy Storage System (BESS)
- Electrical switchgear and associated infrastructure

These improvements are necessary to implement KCAPTA's Zero-Emission Bus Transition Plan and ensure the continued operation of transit services in Kings County.

2. State Regulatory Requirement

Adopted by CARB in December 2018 and effective October 2019, the ICT regulation requires all California public transit agencies to gradually transition their bus fleets to 100% zero-emission buses (ZEBs) by 2040. The regulation applies to all transit agencies and mandates an increasing percentage of new bus purchases each year to be ZEBs, starting in 2023 for large agencies and 2026 for small agencies.

To comply with the regulation, KCAPTA must deploy infrastructure necessary to support electric bus operations.

The requested variances are therefore not discretionary design preferences, but are necessary to enable compliance with state-mandated requirements. Denial of the variance would constrain KCAPTA's ability to implement required infrastructure and continue operating its transit system.

3. Existing Physical and Design Constraints

The KCAPTA facility functions as a bus maintenance and storage yard, which imposes unique operational constraints not typically present on other properties within the same zoning district. Refer to Exhibit A for a diagram of the following constraints:

- A. The project site contains an existing transit maintenance building, which will remain fully operational and is not being modified as part of this project. This building is essential to the maintenance and expansion of KCAPTA's fleet and must remain accessible at all times.

- B. The maintenance bays, circulation routes, and staging areas must remain unobstructed to allow buses to safely enter, exit, and maneuver within the facility.

Transit buses require large turning radii and maneuvering space. As a result, the central portion of the property must remain open to accommodate circulation between bus parking areas, maintenance bays, and site access points. Please see Exhibit B for an accurate illustration of a bus turn, and the existing space limitations.

Due to these constraints, electrification infrastructure must be located along the perimeter of the property.

- C. California Fire Code (CFC) Chapter 23 — CNG Fueling Stations requires the Battery Energy Storage System (BESS) to be located at least 20 feet away from existing compressed natural gas (CNG) storage equipment. This is essential to prevent any accidental ignition.

The combined operational and safety limitations significantly restrict design options.

4. No Feasible Alternative Design

KCAPTA evaluated alternative configurations for locating electrification infrastructure on the property. However, no feasible alternative exists due to the previously presented constraints:

- A. The existing maintenance building must remain fully operational
- B. The central yard must remain open for bus maneuvering
- C. BESS equipment must maintain required separation from CNG storage

Relocating infrastructure outside of setback areas would interfere with bus circulation, compromise safety requirements, or prevent effective charging operations.

The proposed design represents the only feasible configuration that allows KCAPTA to meet operational, safety, and regulatory requirements.

6. Essential Public Infrastructure

The KCAPTA facility is a critical public infrastructure asset that supports transit service throughout Kings County. The facility:

- Serves approximately 715,000 annual riders
- Supports 15 fixed routes, on-demand services, and vanpool operations
- Provides essential mobility for predominantly low-income residents
- Is the only maintenance facility operated by KCAPTA

The proposed improvements do not expand the use of the site, but rather ensure the continued operation of an essential public service under state-mandated electrification requirements.

7. Variance Request – Setback Encroachments

Hanford City’s code specifies that this property should have a 10-foot setback on the north and south boundaries, and a 20-foot setback on the east and west boundaries.

The project requires limited encroachments into the north, east and south setback areas to accommodate solar canopy structures, electrical infrastructure, and bus maneuvering space. As shown on Exhibit A, it necessitates the following encroachments:

North: Canopy 1.6'

East: BESS encroaching 7.2' / Canopy encroaching 17.6'

South: Canopy encroaching 5.8' / Charger encroaching 3.4' / EV Cabinets encroaching 5.1'

The design proposed represents the minimum necessary deviation required to maintain safe operations while implementing electrification infrastructure.

These encroachments are necessary to:

- Maintain safe bus circulation

- Preserve access to the maintenance building
- Comply with safety CNG separation requirements
- Accommodate for projected growth (fleet and ridership)

8. Variance Findings

Finding 1

No property in the vicinity has similar use. There are special circumstances applicable to the property, including size, shape, use, and applicable regulations. The strict application of the local building code would deprive this property from privileges enjoyed by other transit agencies.

Finding 2

The variance is necessary to preserve the property's ability to function as a transit maintenance facility, consistent with other similar properties.

Finding 3

The variance will not be detrimental to public health, safety, or welfare. In fact, the project will help the region reduce carbon emissions from transit fleet, reducing local air pollution, and improving public health prospects. The project specifically, also improves safety through required BESS separation, appropriate maneuvering space, and modernized infrastructure.

Finding 4

The variance does not constitute a special privilege. It enables compliance with state-mandated electrification requirements without increasing development intensity.

Finding 5

The variance does not allow a prohibited use. The site remains a transit maintenance facility.

Finding 6

The variance is consistent with the purpose of the zoning ordinance by supporting safe and efficient public infrastructure.

Finding 7

The project is consistent with the City of Hanford 2035 General Plan, supporting transportation infrastructure, public services, and sustainable operations.

The requested variances are necessary to allow KCAPTA to implement infrastructure required under state law while maintaining the safe and continued operation of an essential public transit facility. Denial of the variance would constrain the agency's ability to comply with CARB's zero-emission mandate and would directly impact a critical public service relied upon by the community.

EXHIBIT A
SITE CONFIGURATION AND CONSTRAINTS

LEGEND

- PROPERTY LINE (RIGHT-OF-WAY)
- SETBACK
- FENCE
- EXISTING SIDEWALK
- EXISTING DRIVEWAY
- GAS LINE
- BUILDING OUTLINE
- CONCRETE PAD (PHASE 1)
- CANOPY (PHASE 1)
- CANOPY (PHASE 2)
- CONCRETE PAVEMENT
- LANDSCAPE
- GRAVEL
- STORM DRAIN STRUCTURE
- SEWER MANHOLE
- FIRE HYDRANT
- CANOPY COLUMN (PHASE 1)
- CANOPY COLUMN (PHASE 2)
- SURVEY MONUMENT

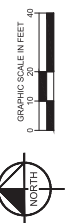
HORIZONTAL CONTROL NOTES (PHASE 1)

1. CONSTRUCT HEAVY DUTY CONCRETE PAVEMENT PER DETAIL 1, SHEET C0.0.
2. CONSTRUCT "THICK BED" CONCRETE ALONG ASPHALT INTERCHANGE PER DETAIL 2, SHEET C0.0.
3. INSTALL GRAVEL SURFACE.
4. CONSTRUCT CONCRETE EQUIPMENT PAD (FOR REFERENCE ONLY - SEE ELECTRICAL PLANS).
5. INSTALL BUS STALL PAVEMENT STRIPING PER DETAIL 3, SHEET C0.0.
6. INSTALL ACCESS RISE PAVEMENT STRIPING PER DETAIL 4, SHEET C0.0.
7. PROPOSED ELECTRICAL EMERGENCY COORDINATION WITH SCE IS ONGOING.
8. REPLACE SALVAGED FENCE.
9. INSTALL UTILITY ACCESS GATE.
10. INSTALL EV CHARGING EQUIPMENT (FOR REFERENCE ONLY - SEE ELECTRICAL PLANS).
11. ELECTRICAL SERVICE EQUIPMENT (FOR REFERENCE ONLY - SEE ELECTRICAL PLANS).
12. INSTALL ELECTRICAL CONDUIT ROUTING (FOR REFERENCE ONLY - SEE ELECTRICAL PLANS).
13. PROPOSED PROTECTION BOLLARD PER DETAIL 5, SHEET C0.0.
14. CONSTRUCT CANOPY COLUMN FOUNDATION (FOR REFERENCE ONLY - SEE CANOPY VENDOR PLANS).
15. INSTALL SOLAR CANOPY (FOR REFERENCE ONLY - SEE CANOPY VENDOR PLANS).
16. PROPOSED PERKINS AREA.
17. CONSTRUCT COMMERCIAL DRIVEWAY PER CITY OF HANFORD DEPARTMENT OF PUBLIC WORKS STANDARD DRAWING C04-11 SEE DETAIL 6, SHEET C0.0.
18. INSTALL NEW ELECTRICAL INSER POLE.

HORIZONTAL CONTROL NOTES (PHASE 2)

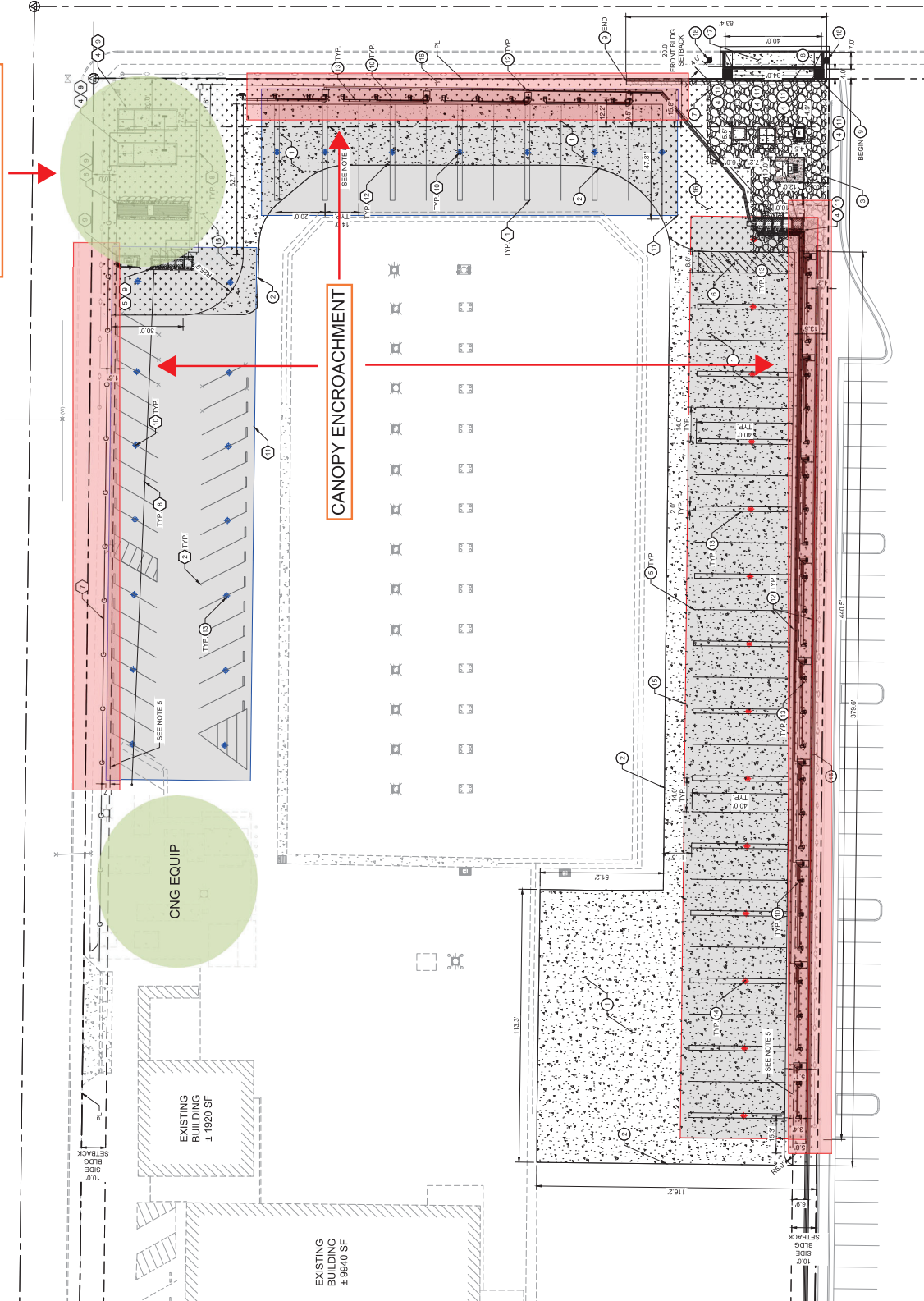
1. PROPOSED BUS STALL PAVEMENT STRIPING.
2. PROPOSED STANDARD PARKING STALL PAVEMENT STRIPING.
3. PROPOSED ELECTRICAL SERVICE EQUIPMENT (FOR REFERENCE ONLY - SEE ELECTRICAL PLANS).
4. PROPOSED GENERATOR (FOR REFERENCE ONLY - SEE ELECTRICAL PLANS).
5. PROPOSED MICROGRID GEAR (FOR REFERENCE ONLY - SEE ELECTRICAL PLANS).
6. PROPOSED ELECTRICAL ENERGY STORAGE SYSTEM (FOR REFERENCE ONLY - SEE ELECTRICAL PLANS).
7. PROPOSED GAS LINE ROUTING (FOR REFERENCE ONLY - SEE MECHANICAL PLANS).
8. PROPOSED ELECTRICAL CONDUIT ROUTING (FOR REFERENCE ONLY - SEE ELECTRICAL PLANS).
9. PROPOSED CONCRETE EQUIPMENT PAD (FOR REFERENCE ONLY - SEE STRUCTURAL PLANS).
10. PROPOSED CANOPY COLUMN FOUNDATION.
11. PROPOSED SOLAR CANOPY.
12. INSTALL EQUIPMENT PROTECTION BOLLARD PER DETAIL 5, SHEET C0.0.

NOTES:
 1. ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
 2. REFER TO ARCHITECTURAL PLANS FOR EXACT BUILDING DIMENSIONS.
 3. REFER TO MECHANICAL PLANS FOR EXACT GAS LINE DIMENSIONS.
 4. HEREIN FOR REFERENCE ONLY. REFER TO CONSTRUCTION PLANS OF THOSE ITEMS FOR LOCATIONS AND DETAILS.
 5. ALL DETAILS ARE PER ARCHITECTURAL PLANS UNLESS OTHERWISE NOTED.
 6. CANOPY EXTENDS INTO BUILDING SETBACK.



BESS - Min. 20' from CNG

CANOPY ENCROACHMENT

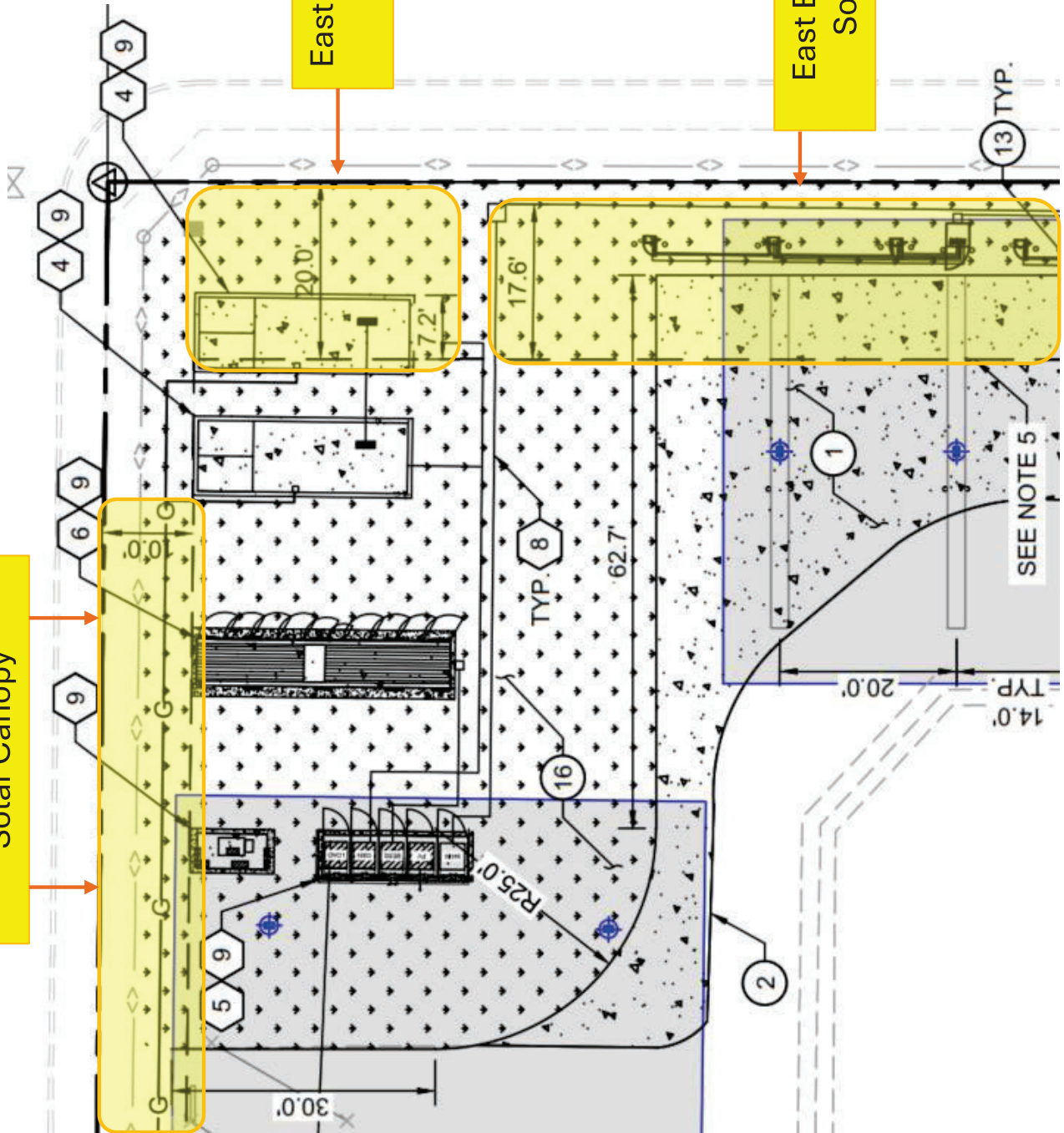


<p>KINGS AREA REGIONAL TRANSIT (KART) ELECTRIC BUS CHARGING 629 W. DAVIS STREET HANFORD, CA 93230 PREPARED FOR ABM eMobility</p>		<p>CITY OF HANFORD</p>	<p>CA</p>
<p>KHA PROJECT NO. 2024-001 DATE 10/22/2025 SCALE AS SHOWN DESIGNED BY CM DRAWN BY AD CHECKED BY ND</p>			
<p>Kimley-Horn</p> <p>© 2025 KIMLEY-HORN AND ASSOCIATES, INC. 475 B STREET, SUITE 600, SAN DIEGO, CA 92101 WWW.KIMLEY-HORN.COM</p>			
<p><small>KIMLEY-HORN SHALL HAVE NO LIABILITY WHATSOEVER FOR ANY COSTS ARISING FROM THE CLIENT'S DECISION TO OBTAIN BIDS. ORDER HAS ISSUED FINAL FULLY-APPROVED PLANS AND SPECIFICATIONS. THE CLIENT ACKNOWLEDGES THAT ALL PRELIMINARY PLANS ARE SUBJECT TO SUBSTANTIAL REVISIONS. ALL PERMITS OBTAINED.</small></p>			
<p>NO. _____</p> <p>REVISIONS _____</p> <p>DATE _____</p> <p>BY _____</p>			
<p>SHEET NUMBER C4.0</p>			

North Encroachment
Solar Canopy

East Encroachment
BESS

East Encroachment
Solar Canopy



SOUTH

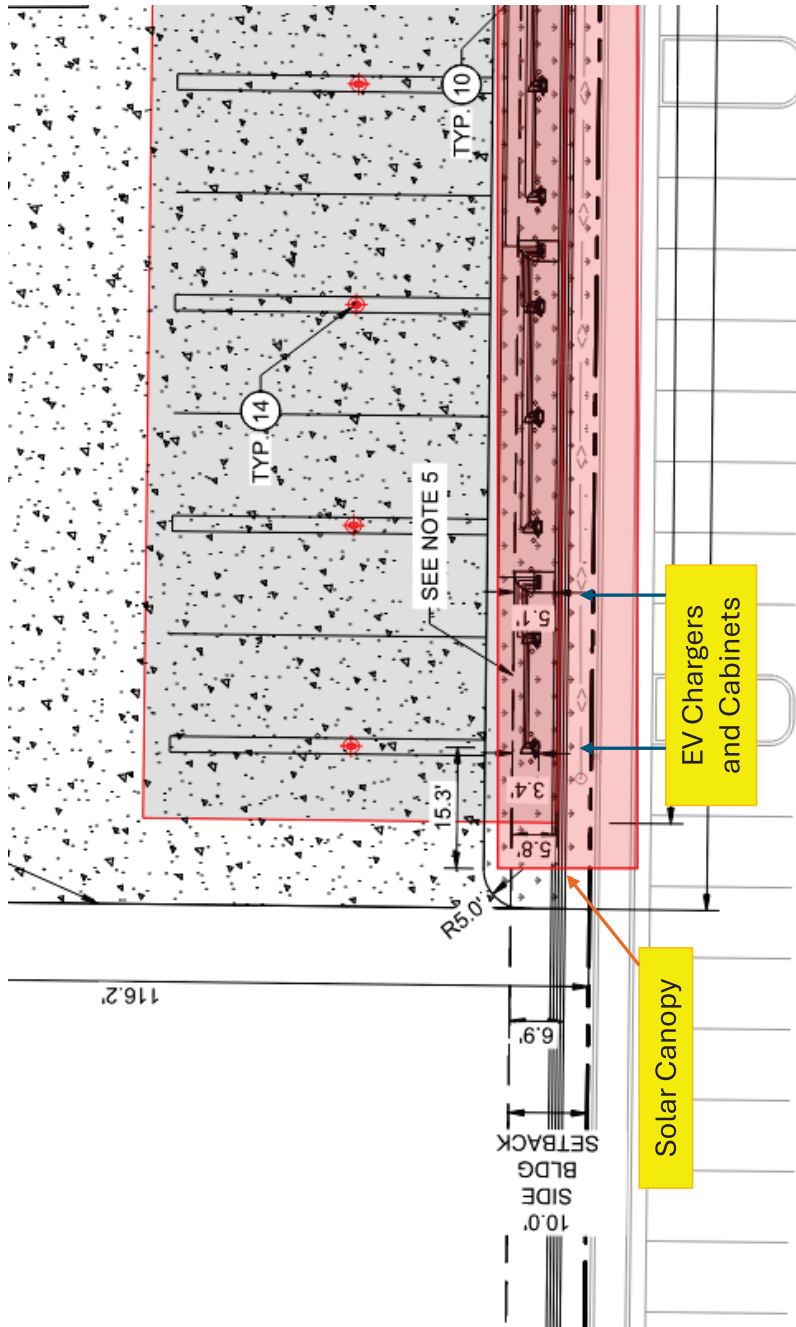


EXHIBIT B
BUS CIRCULATION AND MANEUVERING

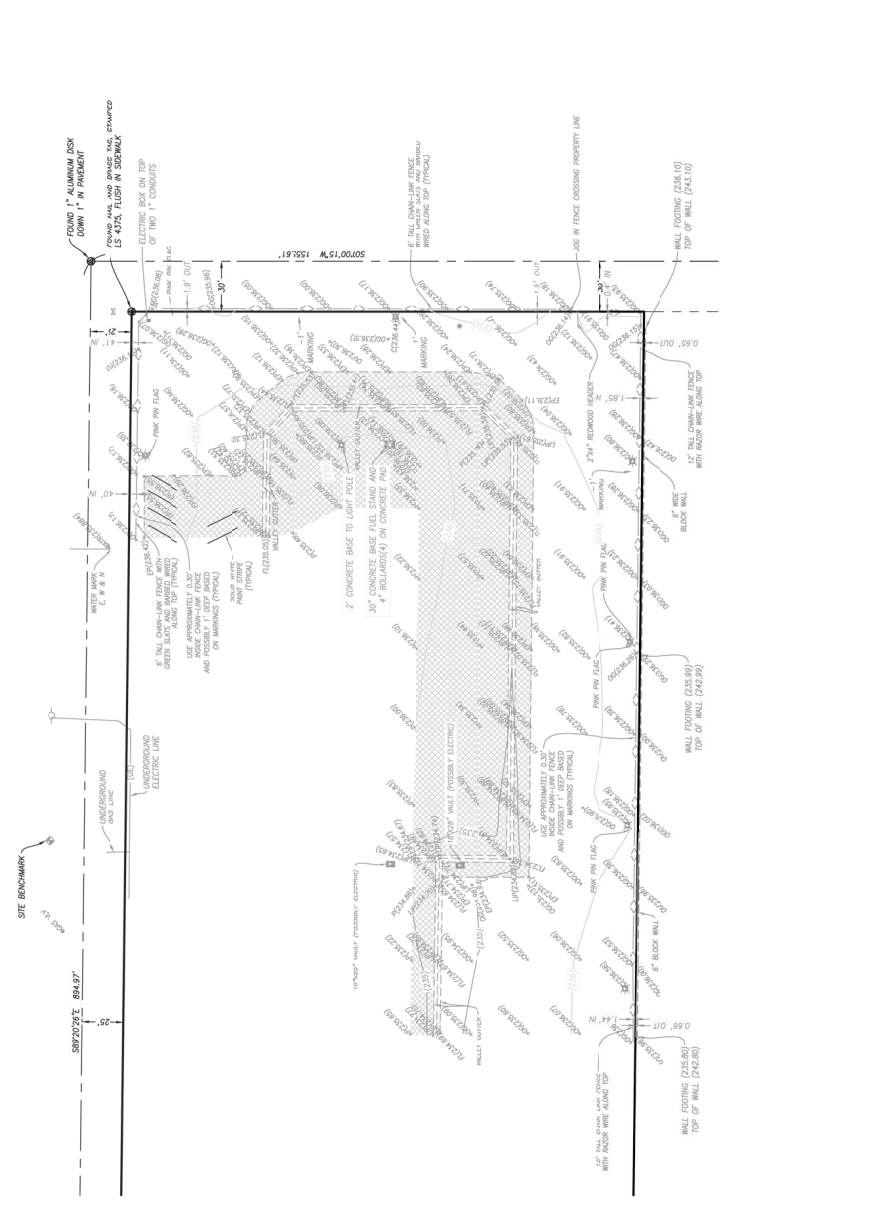
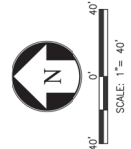
SITE PLAN

TOPOGRAPHICAL SURVEY

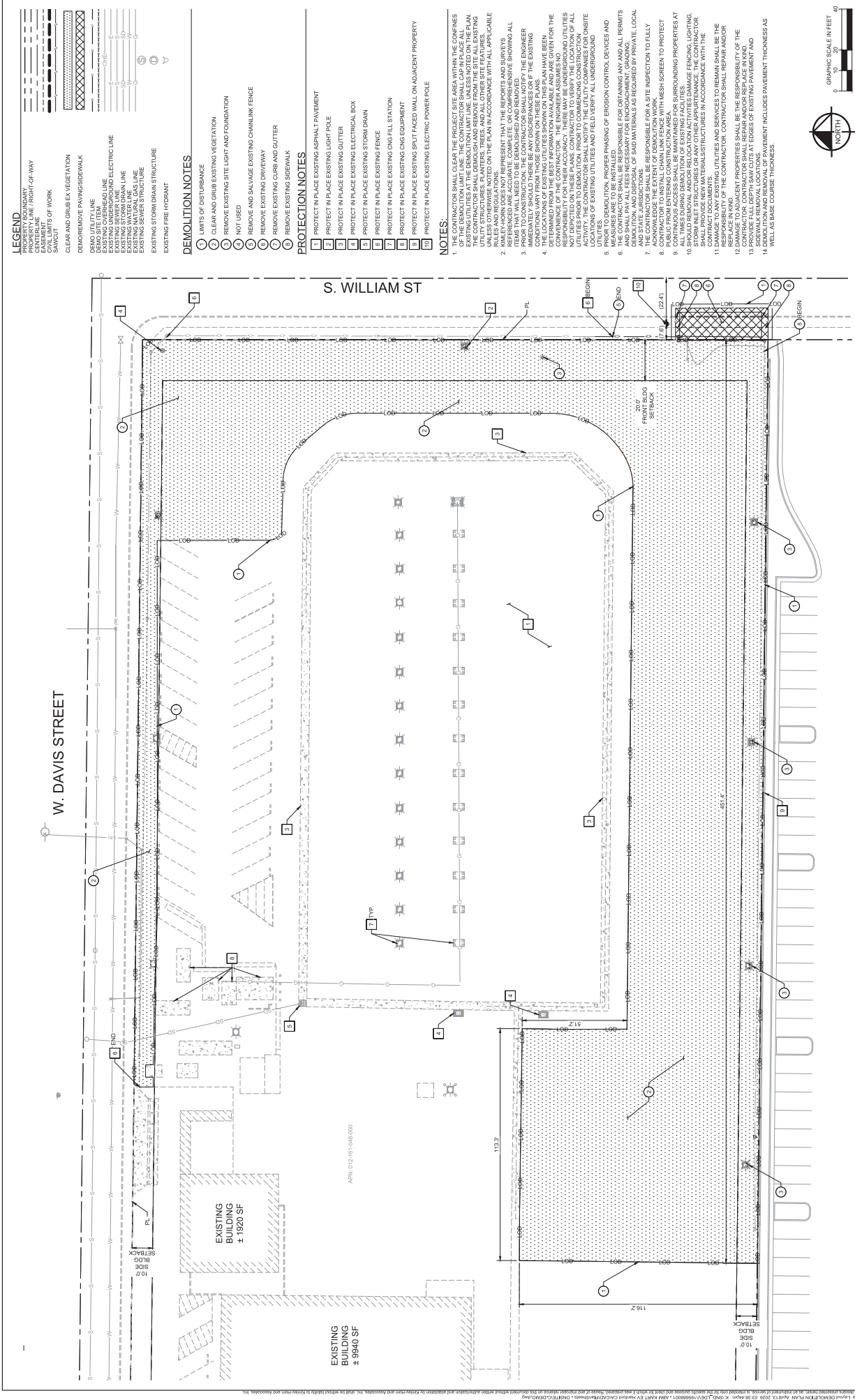
LEGEND
ALL DISTANCES SHOWN ARE IN U.S. SURVEY FEET
AND DECIMALS THEREOF, UNLESS OTHERWISE NOTED.

- BOLLARD
- ELECTRIC BOX
- ELECTRIC MOUNTING
- POWER POLE
- SPRINKLER
- WATER VALVE
- CONCRETE
- EDGE OF PAVEMENT
- FORMLINE
- FOUNDATION
- PAVEMENT
- UNDERGROUND ELECTRIC
- WATER VALVE

BENCHMARK
SEE BM "11" IN THE TOP OF CURB 1' EAST OF THE EAST CURB
RETURN AT THE ROAD FOR FUTURE CONSTRUCTION USE ON THE
PROPERTY BETWEEN 11TH AVE. AND WILLIAMS ST.
E.L. = 235.87



KHA PROJECT DATE 10/22/2025		KINGS AREA REGIONAL TRANSIT (KART) ELECTRIC BUS CHARGING 629 W. DAVIS STREET HANFORD, CA 93230 PREPARED FOR ABM eMobility	SHEET NUMBER G3.0
SCALE: AS SHOWN	DESIGNED BY: CM		
DRAWN BY: AD	CHECKED BY: ND	CITY OF HANFORD	CA
© 2025 KIMLEY-HORN AND ASSOCIATES, INC. 475 B STREET, SUITE 400, SAN DIEGO, CA 92101 WWW.KIMLEY-HORN.COM			
KIMLEY-HORN SHALL HAVE NO LIABILITY WHATSOEVER FOR ANY COSTS ARISING OF THE CLIENTS DECISION TO OBTAIN BIDS. ORDER HAS ISSUED FINAL FULLY-APPROVED PLANS AND SPECIFICATIONS. THE CLIENT ACKNOWLEDGES THAT ALL PRELIMINARY PLANS ARE SUBJECT TO SUBSTANTIAL REVISIONS. ALL PERMITS OR PLANS.			
No.	DATE	BY	



KIMLEY-HORN 475 B STREET, SUITE 400, SAN DIEGO, CA 92101 WWW.KIMLEY-HORN.COM		ABM		KINGS AREA REGIONAL TRANSIT (KART) ELECTRIC BUS CHARGING 629 W. DAVIS STREET HANFORD, CA 93230 PREPARED FOR ABM eMobility		CITY OF HANFORD CA	
KHA PROJECT NO. 10222025 DATE 10/22/2025 SCALE AS SHOWN DESIGNED BY CM DRAWN BY AD CHECKED BY ND		SHEET NUMBER C1.0		DEMOLITION PLAN		GRAPHIC SCALE IN FEET 0 10 20 40 NORTH	

NO.	REVISIONS	DATE	BY

KIMLEY-HORN SHALL HAVE NO LIABILITY WHATSOEVER FOR ANY COSTS ARISING FROM THE CLIENT'S DECISION TO OBTAIN BIDS. ORDER HAS ISSUED FINAL FULLY-APPROVED PLANS AND SPECIFICATIONS. THE CLIENT ACKNOWLEDGES THAT ALL PRELIMINARY PLANS ARE SUBJECT TO SUBSTANTIAL REVISIONS. ALL PERMITS OBTAINED.

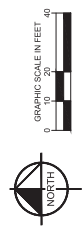
- LEGEND**
- PROPERTY LINE (RIGHT-OF-WAY)
 - CONSTRUCTION FENCE
 - EROSION CONTROL
 - SEED SOAK
 - DIRECTION OF FLOW
 - STORM DRAIN STRUCTURE
 - EXISTING SITE FEATURES
 - STORM DRAIN CURB INLET
 - GRATE INLET
 - STORM DRAIN MANHOLE

APPLICABLE BEST MANAGEMENT PRACTICES
 THE FOLLOWING BMPs AS OUTLINED IN, BUT NOT LIMITED TO, THE CALIFORNIA STORMWATER BMP HANDBOOK DATED JANUARY 2015 OR THE LATEST REVISED EDITION OF THE HANDBOOK SHALL BE USED UNLESS OTHERWISE SPECIFIED. ADDITIONAL MEASURES MAY BE REQUIRED AS NEEDED.

- EC-1. SCHEDULING
- EC-2. PRESERVATION OF EXISTING VEGETATION
- WE-1. WIND EROSION CONTROL
- NS-1. NURTURE NATIVE PLANTS
- NS-2. PAVING AND GRADING OPERATIONS
- NS-3. POTABLE WATER IRRIGATION
- NS-13. CONCRETE FINISHING
- WM-4. SPILL PREVENTION AND CONTROL
- WM-7. CONTAMINATED SOIL MANAGEMENT
- WM-10. LIQUID WASTE MANAGEMENT
- SE-7. STREET SWEEPING AND VACUUMING
- ES-10. INLET PROTECTION
- ES-15. INLET PROTECTION FOR TRUCKS, EQUIPMENT, AND PEDESTRIAN CONTROL WHILE PERFORMING WORK IN THE PUBLIC RIGHT-OF-WAY

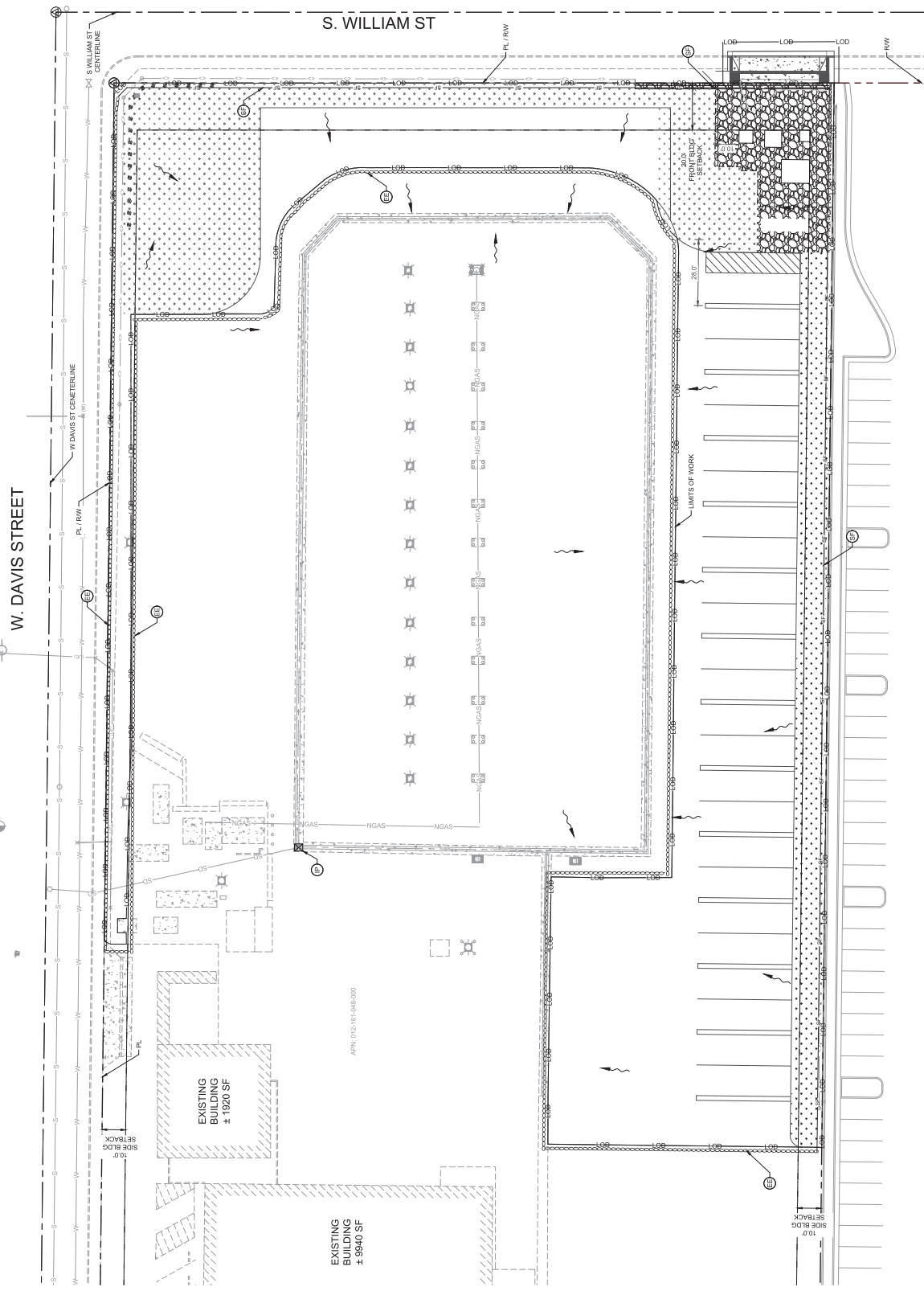
EROSION CONTROL NOTES

- 1. PRESERVATION SHOULD BE IN ACCORDANCE WITH GEOTECHNICAL INVESTIGATION
- 2. CONSULTORS TO USE BEST MANAGEMENT PRACTICES TO ENSURE COMPLIANCE WITH NPDES AND WATER MANAGEMENT DISTRICT REGULATIONS FOR STORMWATER DISCHARGE FROM CONSTRUCTION ACTIVITIES AND DEWATERING OPERATIONS
- 3. EROSION CONTROL FILTER SOCK. SEE DETAIL 1, SHEET C2.0
- 4. PROVIDE TEMPORARY UNDER-GRADE GEOTEXTILE STORM DRAIN INSERT INSTALLED BENEATH THE INLET GRATE IN ACCORDANCE WITH CADA BMP
- 5. SILT FENCE. SEE DETAIL 3, SHEET C2.0



SHEET NUMBER
C2.0

EROSION & SEDIMENT CONTROL PLAN



KINGS AREA REGIONAL TRANSIT (KART)
 ELECTRIC BUS CHARGING
 629 W. DAVIS STREET HANFORD, CA 93230
 PREPARED FOR
 ABM eMobility

CITY OF HANFORD CA

ABM

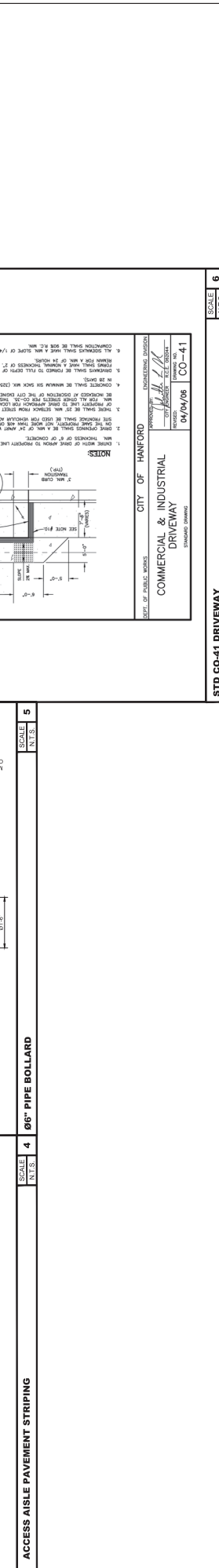
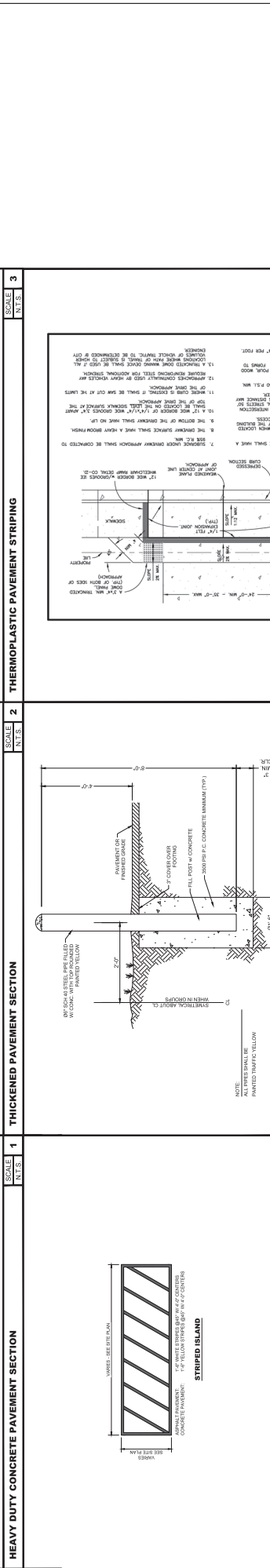
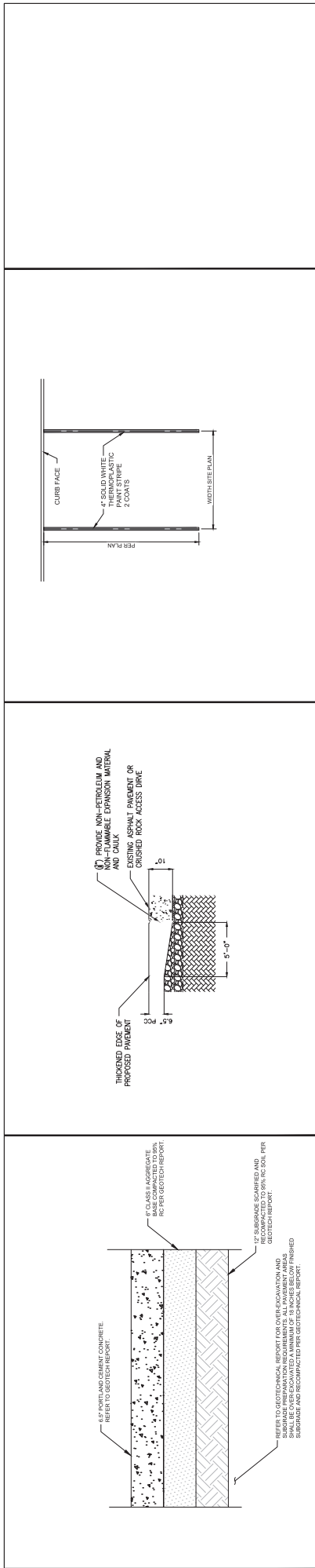
KHA PROJECT NO.	DATE	AS SHOWN	DESIGNED BY	AD	CHECKED BY	MD
10222025	10/22/2025	AS SHOWN	CM	AD		

Kimley»Horn

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 WWW.KIMLEY-HORN.COM

NO.	REVISIONS	DATE	BY

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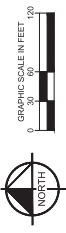
<p>NOTES</p> <ol style="list-style-type: none"> 1. FORM WITH 2" OR MORE TO INTERIOR LINE SHALL HAVE A MIN. THICKNESS OF 4" OF CONCRETE TO BE CAST IN PLACE. 2. THE SPACING SHALL HAVE A MIN. THICKNESS OF 1/4" PER FOOT. 3. THE SPACING SHALL BE MINIMUM 1/4" FROM THE FACE OF THE CURB OR FROM THE FACE OF THE CURB TO THE FACE OF THE CURB. 4. CONCRETE SHALL BE MINIMUM 4000 PSI (MIN. 70% F'c). 5. THE SPACING SHALL BE MINIMUM 1/4" FROM THE FACE OF THE CURB OR FROM THE FACE OF THE CURB TO THE FACE OF THE CURB. 6. ALL SPACING SHALL HAVE A MIN. THICKNESS OF 1/4" PER FOOT. 7. THE SPACING SHALL BE MINIMUM 1/4" FROM THE FACE OF THE CURB OR FROM THE FACE OF THE CURB TO THE FACE OF THE CURB. 8. THE SPACING SHALL BE MINIMUM 1/4" FROM THE FACE OF THE CURB OR FROM THE FACE OF THE CURB TO THE FACE OF THE CURB. 9. THE SPACING SHALL BE MINIMUM 1/4" FROM THE FACE OF THE CURB OR FROM THE FACE OF THE CURB TO THE FACE OF THE CURB. 10. THE SPACING SHALL BE MINIMUM 1/4" FROM THE FACE OF THE CURB OR FROM THE FACE OF THE CURB TO THE FACE OF THE CURB. 11. THE SPACING SHALL BE MINIMUM 1/4" FROM THE FACE OF THE CURB OR FROM THE FACE OF THE CURB TO THE FACE OF THE CURB. 12. THE SPACING SHALL BE MINIMUM 1/4" FROM THE FACE OF THE CURB OR FROM THE FACE OF THE CURB TO THE FACE OF THE CURB. 13. THE SPACING SHALL BE MINIMUM 1/4" FROM THE FACE OF THE CURB OR FROM THE FACE OF THE CURB TO THE FACE OF THE CURB. 14. THE SPACING SHALL BE MINIMUM 1/4" FROM THE FACE OF THE CURB OR FROM THE FACE OF THE CURB TO THE FACE OF THE CURB. 15. THE SPACING SHALL BE MINIMUM 1/4" FROM THE FACE OF THE CURB OR FROM THE FACE OF THE CURB TO THE FACE OF THE CURB. 16. THE SPACING SHALL BE MINIMUM 1/4" FROM THE FACE OF THE CURB OR FROM THE FACE OF THE CURB TO THE FACE OF THE CURB. 17. THE SPACING SHALL BE MINIMUM 1/4" FROM THE FACE OF THE CURB OR FROM THE FACE OF THE CURB TO THE FACE OF THE CURB. 18. THE SPACING SHALL BE MINIMUM 1/4" FROM THE FACE OF THE CURB OR FROM THE FACE OF THE CURB TO THE FACE OF THE CURB. 19. THE SPACING SHALL BE MINIMUM 1/4" FROM THE FACE OF THE CURB OR FROM THE FACE OF THE CURB TO THE FACE OF THE CURB. 20. THE SPACING SHALL BE MINIMUM 1/4" FROM THE FACE OF THE CURB OR FROM THE FACE OF THE CURB TO THE FACE OF THE CURB. 		<p>SCALE N.T.S.</p> <p>3</p>
<p>NOTES</p> <ol style="list-style-type: none"> 1. PROVIDE NON-PETROLEUM AND NON-SOLUBLE EXPANSION MATERIAL AND CALK UNDER THE CURB AND UNDER THE EXISTING ASPHALT PAVEMENT OR CRUSHED ROCK ACCESS DIRK. 		<p>SCALE N.T.S.</p> <p>2</p>
<p>NOTES</p> <ol style="list-style-type: none"> 1. PROVIDE NON-PETROLEUM AND NON-SOLUBLE EXPANSION MATERIAL AND CALK UNDER THE CURB AND UNDER THE EXISTING ASPHALT PAVEMENT OR CRUSHED ROCK ACCESS DIRK. 		<p>SCALE N.T.S.</p> <p>5</p>
<p>NOTES</p> <ol style="list-style-type: none"> 1. PROVIDE NON-PETROLEUM AND NON-SOLUBLE EXPANSION MATERIAL AND CALK UNDER THE CURB AND UNDER THE EXISTING ASPHALT PAVEMENT OR CRUSHED ROCK ACCESS DIRK. 		<p>SCALE N.T.S.</p> <p>4</p>
<p>NOTES</p> <ol style="list-style-type: none"> 1. PROVIDE NON-PETROLEUM AND NON-SOLUBLE EXPANSION MATERIAL AND CALK UNDER THE CURB AND UNDER THE EXISTING ASPHALT PAVEMENT OR CRUSHED ROCK ACCESS DIRK. 		<p>SCALE N.T.S.</p> <p>6</p>

PROPERTY BOUNDARY MAP

LEGEND

- PROPERTY LINE (RIGHT-OF-WAY)
- PROPERTY LINE
- SETBACK
- FENCE
- EXISTING DRIVEWAY
- EXISTING SITE FEATURES
- GAS LINE
- BUILDING OUTLINE
- LANDSCAPE (PHASE 1)
- CANOPY (PHASE 1)
- CANOPY (PHASE 2)
- CONCRETE PAVEMENT
- LANDSCAPE
- GRAVEL
- STORM DRAIN STRUCTURE
- SEWER MANHOLE
- FIRE HYDRANT
- CANOPY COLUMN (PHASE 1)
- CANOPY COLUMN (PHASE 2)
- SURVEY MONUMENT

- NOTES:**
1. ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
 2. REFER TO ARCHITECTURAL PLANS FOR EXACT BUILDING DIMENSIONS.
 3. REFER TO ARCHITECTURAL PLANS FOR EXACT BUILDING DIMENSIONS.
 4. HEREIN FOR REFERENCE ONLY. REFER TO CONSTRUCTION PLANS OF THOSE ITEMS FOR LOCATIONS AND DETAILS.
 5. OTHER DETAILS ARE PER ARCHITECTURAL PLANS UNLESS OTHERWISE NOTED.
 6. CANOPY EXTENDS INTO BUILDING SETBACK.



KIMLEY-HORN AND ASSOCIATES, INC. 475 B STREET, SUITE 600, SAN DIEGO, CA 92101 WWW.KH-CA.COM		KINGS AREA REGIONAL TRANSIT (KART) ELECTRIC BUS CHARGING 629 W. DAVIS STREET HANFORD, CA 93230 PREPARED FOR ABM eMobility		CITY OF HANFORD CA	
K/A PROJECT NO.	DATE	SCALE	AS SHOWN	DESIGNED BY	CM
10222025	10/22/2025	AS SHOWN	AS SHOWN	AD	AD
CHECKED BY	ND	PROPERTY MAP EXHIBIT			
SHEET NUMBER					
1					

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NO.	REVISIONS	DATE	BY

MAILING LABELS



A) Leroy & Linda Taylor
 603 S 11th Ave
 Hanford CA 93230

B) Orlando Rodriguez & Karina Amaya
 605 s 11th Ave
 Hanford CA 93230

C) Martin & Luz Dediosedeniz
 607 S 11th Ave
 Hanford CA 93230

D) Premier Z LLC 609 S 11 th Ave Hanford CA 93230	Mailing Address 520 W Lacey Blvd , Ste 9a Hanford CA 93230
--	--

E) Hanford Pacific Associates Tierra Vista Apartments 731 S 11 th Ave Hanford CA 93230	Mailing Address 430 E State St, Ste 100 Eagle, ID 83616
--	---

F) Penny Newman Grain CO 619 S Williams St. Hanford CA 93230	Mailing Address PO Box 12147 Fresno CA 93776
--	--

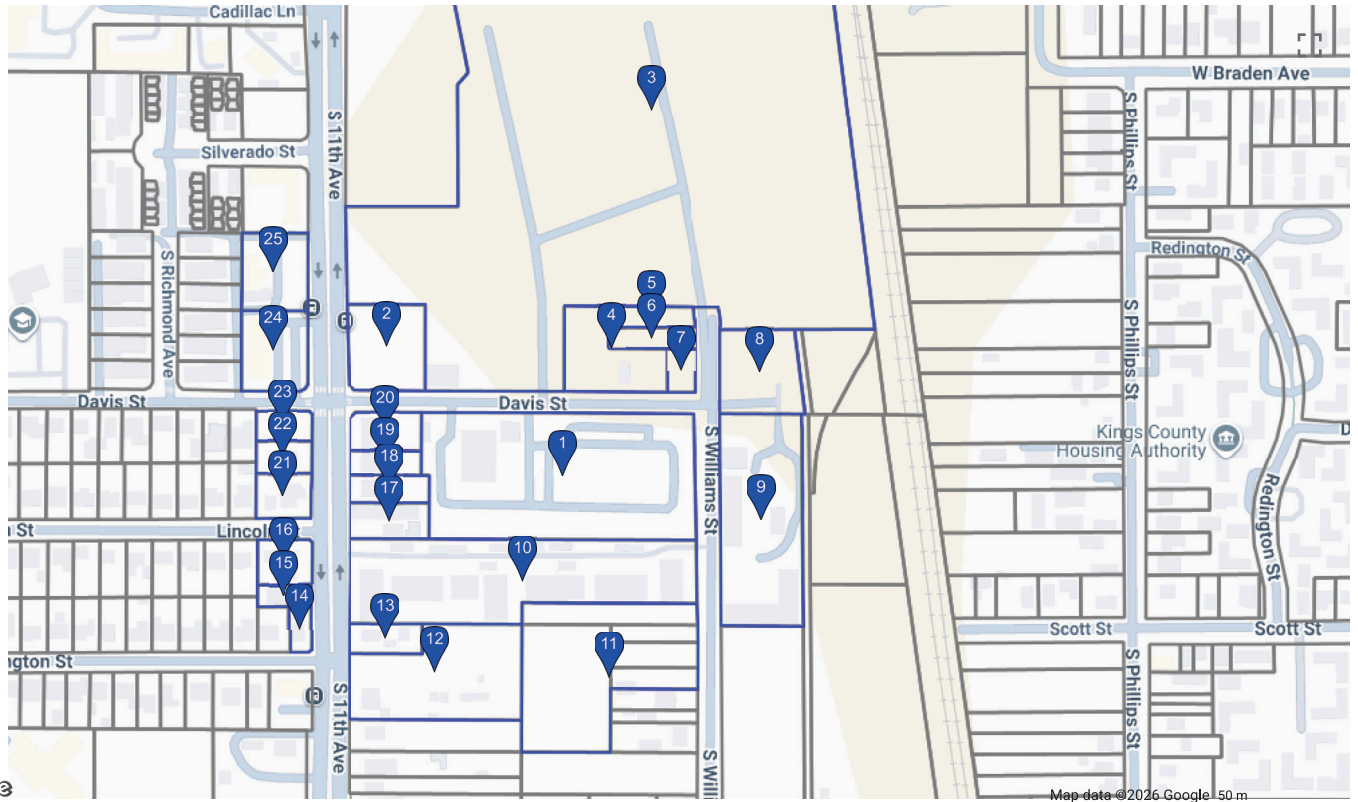
G) Hanford Commodities LLC
10700 Hanford Armona Rd
Hanford CA 93230

Mailing Address
13684 Francisco Dr
Hanford CA 93230

H) Mario Ortega
622 Davis St
Hanford CA 93230

I) Valero Energy Corp
527 S 11th Ave
Hanford CA 93230

Mailing Address
PO Box 690110
San Antonio TX 78269



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Map data © 2026 Google 50 m



<input checked="" type="checkbox"/>		Co	APN	Owner	S Street Address	S City State Zip
<input checked="" type="checkbox"/>	<u>1</u>	KIN	012-161-048-000	KINGS COUNTY AREA PUBLIC TRANSIT AGENCY	629 DAVIS ST	HANFORD CA 93230-5307
<input checked="" type="checkbox"/>	<u>2</u>	KIN	012-151-011-000	VALERO ENERGY CORPORATION	527 S 11TH AVE	HANFORD CA 93230-5356
<input checked="" type="checkbox"/>	<u>3</u>	KIN	012-151-014-000	VALERO ENERGY CORPORATION	525 W THIRD ST	HANFORD CA 93230-5016
<input checked="" type="checkbox"/>	<u>4</u>	KIN	012-151-012-000	ORTEGA MARIO	622 DAVIS ST	HANFORD CA 93230-5308
<input checked="" type="checkbox"/>	<u>5</u>	KIN	012-151-009-000	VALERO ENERGY CORPORATION	528 S WILLIAMS ST	HANFORD CA 93230
<input checked="" type="checkbox"/>	<u>6</u>	KIN	012-151-008-000	VALERO ENERGY CORPORATION	532 S WILLIAMS ST	HANFORD CA 93230
<input checked="" type="checkbox"/>	<u>7</u>	KIN	012-151-006-000	VALERO ENERGY CORPORATION	605 DAVIS ST	HANFORD CA 93230-5307
<input checked="" type="checkbox"/>	<u>8</u>	KIN	012-151-019-000	PENNY NEWMAN GRAIN CO		
<input checked="" type="checkbox"/>	<u>9</u>	KIN	012-162-011-000	PENNY-NEWMAN GRAIN CO	619 S WILLIAMS ST	HANFORD CA 93230-5344
<input checked="" type="checkbox"/>	<u>10</u>	KIN	012-161-054-000	HANFORD PACIFIC ASSOCIATES	613 S 11TH AVE	HANFORD CA 93230-5358
<input checked="" type="checkbox"/>	<u>11</u>	KIN	012-161-059-000	LONG TERM LAND HOLDINGS		
<input checked="" type="checkbox"/>	<u>12</u>	KIN	012-161-029-000	TORRES BARBARA E 50%	813 S 11TH AVE	HANFORD CA 93230-5362
<input checked="" type="checkbox"/>	<u>13</u>	KIN	012-161-030-000	FLORES, LORENZO V	811 S 11TH AVE	HANFORD CA 93230-5362
<input checked="" type="checkbox"/>	<u>14</u>	KIN	012-323-016-000	PINEDO, EUGENIO R	792 S 11TH AVE	HANFORD CA 93230-5361
<input checked="" type="checkbox"/>	<u>15</u>	KIN	012-323-014-000	RODRIGUEZ, JAVIER A & ESTHER H/W 66% W 66%	750 S 11TH AVE	HANFORD CA 93230-5361
<input checked="" type="checkbox"/>	<u>16</u>	KIN	012-323-013-000	FLYLINE INVESTMENTS LLC	702 S 11TH AVE	HANFORD CA 93230-5361
<input checked="" type="checkbox"/>	<u>17</u>	KIN	012-161-041-000	Z PREMIER LLC	609 S 11TH AVE	HANFORD CA 93230-5370
<input checked="" type="checkbox"/>	<u>18</u>	KIN	012-161-040-000	DEDIOSDENIZ, MARTIN & LUZ E D H/W	607 S 11TH AVE	HANFORD CA 93230-5358
<input checked="" type="checkbox"/>	<u>19</u>	KIN	012-161-035-000	RODRIGUEZ ORLANDO H & AMAYA KARINA C M	605 S 11TH AVE	HANFORD CA 93230-5358
<input checked="" type="checkbox"/>	<u>20</u>	KIN	012-161-001-000	TAYLOR, LEROY & LINDA H/W	603 S 11TH AVE	HANFORD CA 93230-5358
<input checked="" type="checkbox"/>	<u>21</u>	KIN	012-322-013-000	BRANCHE, REON 50%	708 LINCOLN ST	HANFORD CA 93230-5648
<input checked="" type="checkbox"/>	<u>22</u>	KIN	012-322-012-000	HARMON WILLIAM & EMILY	650 S 11TH AVE	HANFORD CA 93230-5359
<input checked="" type="checkbox"/>	<u>23</u>	KIN	012-322-011-000	BARBA, FRANK S 90%	602 S 11TH AVE	HANFORD CA 93230-5359
<input checked="" type="checkbox"/>	<u>24</u>	KIN	012-332-014-000	NUMI, HASHIM	580 S 11TH AVE	HANFORD CA 93230-5357
<input checked="" type="checkbox"/>	<u>25</u>	KIN	012-332-013-000	MARTELLA LAWRENCE W & RUTH E REV LIV TRUST	540 S 11TH AVE	HANFORD CA 93230-5357



DATE: April 30, 2026

PROJECT: Site Plan Review SPR0170-26 Revision 1

APPLICANT: Alexander Dubney

LOCATION: 629 W. Davis St. (APN 012-161-048)

PROPOSAL: Expansion of the existing KART bus facility

ZONING: PF Public Facilities

SITE PLAN REVIEW COMMITTEE REVIEW DATE: February 4, 2026; April 22, 2026

Enclosed for your review are the comments and decisions of the Site Plan Review Committee. Please review all comments, since they may impact your project:

- RESUBMIT:** Major changes to your plans are required prior to site plan approval.
- PROCEED:** Plans approved subject to the attached conditions of approval, including the revision of any stated plan corrections. The following department comments and conditions are attached:

- Planning - conditions
- Fire
- Other:
- Building
- Public Works - conditions

- Submit plans for a building permit via the Citizen Self Service Portal.
- To complete the entitlement process, your plans must be reviewed by:

- City Council
- Parking and Traffic Commission
- Other: _____
- Planning Commission - Variance
- Parks and Recreation Commission

Signed,

Maddison Faria

Maddison Faria
Assistant Planner, Community Development Department

April 30, 2026

DATE



EXPIRATION

A site plan approval shall expire one (1) year from its approval date, unless a building permit application has been submitted or the use has commenced. Prior to expiration, the Community Development Director may extend the permit expiration date by one (1) year if there is substantial evidence that the applicant is diligently pursuing building permit approval or commencement of the use (Hanford Municipal Code Section 17.72.070).

CALIFORNIA ENVIRONMENTAL QUALITY ACT

Site Plan Review is a ministerial action, therefore not subject to the California Environmental Quality Act (CEQA) (CEQA Guidelines 15268 and MC 17.70.070 and 17.70. 080). **Exemption No. 2026-04**

DEFENSE AND INDEMNIFICATION PROVISION

That the applicant shall defend, indemnify, and hold harmless the City of Hanford (“City”), its officials, officers, employees, representatives, agents and attorneys, from and against all claims, damages, losses, judgments, liabilities, expenses and other costs, including litigation costs and attorney’s fees, arising out of, resulting from, or in connection with, the City’s act or acts leading up to and including approval of any environmental document and/or granting of any approvals relating to the Project. Applicant’s obligation to defend, indemnify and hold the City harmless specifically includes, but is not limited to, any suit or challenge by any third party against the City which challenges or seeks to set aside, void or annul the legality or adequacy of any environmental document approved by the City or any approval related to the Project.

APPLICANT'S STATEMENT (Must be signed prior to issuance of a building permit)

I have reviewed a copy of the Site Plan Review Approval SPR0170-26 Revision 1 and I understand that the proposed construction and/or land use is dependent upon the fulfillment of the conditions of approval and construction of all required public improvements prior to use, inauguration, or occupancy, unless other arrangements are approved.

Applicant or Authorized Representative

PLANNING DIVISION
SITE PLAN REVIEW COMMENTS

MEETING DATE: February 4, 2026; April 22, 2026

SITE PLAN NUMBER: SPR0170-26 REVISION 1

CONTACT: Maddison Faria, Assistant Planner: (559) 415-7183, mfarria@hanfordca.gov

General Plan Designation: Public Facilities

Zoning: PF Public Facilities

Other Special Districts: N/A

Planning Division Recommendation: Proceed Revise and Resubmit

The project meets the requirements of the Hanford Municipal Code, development standards, and other codes and policies.

Correction(s):

- N/A

Required Entitlements:

- Approval of Variance (VAR0031-26) to encroach into required setback areas.

PF Public Facilities - all checked comments shall apply

General:

- That approval of this project does not exempt compliance with all applicable sections of the Zoning Ordinance, Public Works Improvement Standards, fees, or other City Ordinances.
- That all approved proposals of the applicant be conditions of development, if not mentioned herein.
- That the site be developed according to the approved site plan, titled Site Plan Review SPR0170-26 Revision 1 with minor modifications to be approved by the Community Development Department.
- That no expansion of the use which would tend to increase the projected scale of operations beyond the scope and nature described in this Site Plan Review shall be permitted except upon application for, and approval of, modification of this application according to all procedures and requirements thereof.

Cultural Resources:

- That if cultural resources are discovered during construction or related activities, all work shall be halted and a qualified archeologist and the City of Hanford shall be notified. The find shall be properly investigated and appropriate measures are to be taken before construction may continue.

Coverage: (Section 17.38.050)

- That the maximum coverage of a lot shall be determined by the combined building setback area requirements, accessory structure limitations, open space requirements, and off-street parking requirements.

Building Setback Area: (Section 17.38.060)

- That setbacks shall be measured as prescribed in Section 17.50.160.
- That no structure shall be placed within a building setback area.
- The front building setback area shall be 20 feet from the front lot line.
- That the rear building setback shall be 15 feet from the rear lot line, except where there is a landscape easement with a wall or fence on the rear of the lot, then the rear building setback shall be 20 feet.
 - The rear building setback area shall be increased by 10 feet for buildings over one story high.
- That the side building setback area shall be five feet from an interior side lot line and 10 feet from a street side property line.
- That where there is a landscape easement with a wall or fence on the street side of the lot the side yard setback area shall be measured from the easement area instead of the side lot line.
- That exceptions shall be allowed as prescribed in Section 17.50.170.

Distance between Structures: (Section 17.38.070)

- The minimum distance between structures shall be 10 feet, except as provided by the building code.

Height of Structures: (Section 17.38.080)

- The maximum structure height shall be 50 feet.

Driveways: (Section 17.38.090)

- That wherever possible developments shall share driveways to minimize the number of driveways on public streets.
- That new driveways near street corners shall be located a minimum 100 feet from the radius curve of the curb, unless otherwise specifically approved by the City Engineer.
- That gated entrances shall be set back at least 50 feet from the public right-of-way, unless a shorter distance is approved by the City Engineer.

Off Street Parking: (Section 17.38.110 and 17.54)

- That all existing parking on site shall be maintained.

- That required parking spaces may not be provided in within any front, side, or rear building setback area.
- That the standard parking space shall not be less than 18.5 feet in length and nine feet in width, exclusive of aisles and access drives (Section 17.54.110 A).
- That the maximum number of compact-car parking spaces is limited to 30 percent of the total parking spaces. A compact parking space shall not be less than 16 feet in length and 8 feet in width, and marked for compact cars (Section 17.54.110 B. and C).
- There shall be no more than four compact spaces adjacent to each other.
- That parking for bicycles and low-emission vehicles shall be provided in accordance with the latest adopted version of the California Building Code (Section 17.54.190).
- That parking lots with forty (40) or more spaces may substitute standard parking spaces with motorcycle spaces if they are located within one hundred fifty (150) feet of a building entry. One (1) standard space may be replaced with a motorcycle space for each forty (40) required spaces, up to a maximum of two-and-one-half (2.5) percent of the total number of parking spaces.
- Motorcycle spaces shall be a minimum size of four (4) feet by eight (8) feet.
- That all parking spaces, whether required by this chapter or in addition that which is required, shall be located on a lot or parcel behind the front building setback and outside of a street side yard setback, except that vehicles may park on a paved driveway in the front building setback area. This requirement applies to both covered and uncovered parking areas.
- That all parking areas shall have ingress and egress to and from a street or alley as required by the City's standard specifications.
- That sufficient room for turning and maneuvering vehicles shall be provided on the site.
- That developed parking areas are to be utilized by all vehicles associated with or visiting the site.
- That the parking of vehicles on lawn, landscaped areas, or other areas not designed for parking are prohibited.
- That entrances and exits to parking lots and other parking facilities shall be provided only at locations approved by the Site Plan Review Committee.
- That parking lot lighting shall be deflected away from adjoining sites so as not to cause glare to such sites.
- That no commercial repair work, washing or servicing of vehicles shall be conducted in a parking area.
- That parking areas, aisles, and access drives shall be paved with a solid material so as to provide a durable, dustless surface and shall be so graded and drained as to dispose of surface water, except that long-term storage areas for vehicles in the C-S, I-L and I-H zone districts may be surfaced with rock, gravel, granite or solid paving.
- That parking areas shall be designed to the City's standard specifications and approval by the City Engineer.

Landscaping: (Sections 17.38.130 and 17.52)

- That landscaping shall be provided for each use as prescribed in Section 17.52 Landscape Standards.
- That except for driveways and approved parking areas, all yard areas and setback areas visible from a public street shall be landscaped with live plant materials and ornamentation common to the Hanford area or up to 100% of the yard and setback area may be landscaped with artificial turf and other permeable surfaces.

Fencing and Walls: (Sections 17.38.140, 17.50.110, and 17.50.120)

- That fences and walls in the R-L, R-M, R-H, OR, O and **PF** zone districts shall be constructed or installed in accordance with the following:
 - Along a rear lot line not exceeding seven (7) feet in height.
 - Along an interior side lot line not exceeding seven (7) feet in height beginning at ten (10) feet from the front lot line.
 - On a corner lot along the street side lot line not exceeding seven (7) feet in height beginning at twenty-five (25) feet from the front lot line. On a reverse corner lot, the fence or wall is to be set back from the street side lot line five (5) feet. On a reverse corner lot, the rear lot line fence is not to extend beyond the five (5) foot street side building setback area.
 - On a corner lot when a residence is constructed with its front access facing the street side lot line of a corner lot, a six (6) foot high fence is to be placed ten (10) feet behind the front property line. The street corner forty-five (45) degree angle line of sight is to be maintained as described in subsection H of this section. When the front of the house fronts the street side lot line a fence exceeding three (3) or four (4) feet in height as described in subsections E and F of this section is to be set back ten (10) feet from the street side lot line.
 - Along a front lot line not exceeding three (3) feet in height.
 - Along a front lot line not exceeding four (4) feet in height when the upper one (1) foot is fifty percent (50%) or more open.
 - Along a front lot line an open chain link fence up to seven (7) feet in height may be located in conjunction with a public or quasi-public playground or park use.
 - A six (6) foot fence or wall may be located in a front building setback area when set back ten (10) feet from the front lot line. Except in the case of a corner lot, the fence wall shall begin a forty-five (45) degree angle at a point twenty-five (25) feet from the front lot line located on the street side lot line.
- That any proposed fencing on the site be maintained in good repair.
- That no fence or wall shall be placed within the public right-of-way.
- That a masonry fence exceeding three (3) feet in height shall require engineered footings and a building permit. All other fencing exceeding seven (7) feet in height shall require engineered post footings and a building permit.

- That no hedge, shrub, fence, or wall exceeding the three (3) feet in height, or four (4) feet in height if the upper one (1) foot is fifty (50) percent or more open shall be planted, placed, or maintained within the twenty-five (25) feet corner sight triangle of a corner lot, or along a front or street side property line where the hedge, shrub, fence, or wall creates a traffic or pedestrian hazard as determined by the Community Development Director.
- That a block wall with a minimum height of seven feet shall be provided along any side or rear lot line that abuts an R-L, R-M, R-H, OR, AP, or CO zone district.
- That no fence or wall shall be placed in front of or within any landscaped area located next to a street.
- That the open storage of materials and equipment attendant to a use shall be permitted only within an area surrounded or screened by a solid wall or fence seven feet minimum in height. Such storage shall not be visible above the fence or wall.

Outdoor Lighting Standards: (Section 17.50.140)

- That all lights and light fixtures, except public street lights, shall be located, aimed or shielded so as to minimize light trespassing across property boundaries or skyward.
- That no lights or light fixtures shall flash, revolve, blink or otherwise resemble a traffic control signal or operate in such a fashion to create a hazard for passing traffic.
- That building mounted lighting fixtures shall be attached only to the walls of the building. The top of a light fixture attached to a building wall shall not be higher than the top of the building parapet or the top of the roof eave, whichever is lower.
- That canopy ceiling light fixtures shall be recessed or the sides of the lens area shall be shielded in order to eliminate emission of horizontal light.
- That the height of freestanding light fixtures, including freestanding parking lot fixtures, shall be measured from the top of a light fixture to the adjacent grade at the base of the support for that light fixture and shall not exceed:
 - Eighteen (18) feet in height, when located within fifty (50) feet of any residential zone district.
 - Twenty-five (25) feet in height when located within fifty-one (51) to one hundred fifty (150) feet of any residential zone district.
 - Thirty (30) feet in height when located more than one hundred fifty (150) feet from any residential zone district.
 - Fifty (50) feet in height when located in the C-R Regional Commercial zone or freestanding light fixtures for public outdoor recreational facilities, when not located within 150 feet of any residential zone.
- That the following additional requirements shall apply to outdoor lighting fixtures in the O, PF, and all commercial and mixed use zone districts:

- All outdoor fixture lighting shall be a fully shielded fixture and focused to minimize light trespass and glare.
- Outdoor lighting fixtures shall be turned off after close-of-business unless needed for safety or security, in which case the lighting shall be activated by motion sensor devices. Illuminated signs and parking lot lighting are excluded from this requirement.
- Lighting for signs, recreational facilities and decorative effects for building and landscape shall be fully shielded fixtures equipped with automatic timing devices and focused to minimize light glare and light trespass.

Noise:

- That construction equipment is muffled and construction activities be limited to the hours between 7:00 a.m. to 10:00 p.m., Monday through Friday, unless the construction is within the enclosed structure or approved by the Community Development Department.
- That noise from fixed mechanical equipment, when measured at the property line, meets the standard of the Hanford Noise Element.

Dust Control:

- That the appropriate dust-control practices of the San Joaquin Valley Air Pollution Control District shall be implemented.
- That the any necessary permits be obtained through the San Joaquin Valley Air Pollution Control District.

H PUBLIC WORKS DIVISION AND UTILITIES & ENGINEERING DIVISION DEVELOPMENT PLAN REQUIREMENTS

<input type="checkbox"/>	ADRIAN RUBALCABA (SENIOR ENGINEER)	(559) 585-2596
<input type="checkbox"/>	STEVE COODEY (ASSISTANT ENGINEER)	(559) 585-2582
<input checked="" type="checkbox"/>	ROBBIE BUCHANAN (ASSISTANT ENGINEER)	(559) 585-2556
<input type="checkbox"/>	KATLIN ROWEY (ASSOCIATE SURVEYOR)	(559) 415-7188

PROJECT NO.:	<i>SPR0170-26</i>
DATE:	<i>04/23/26</i>
PROJECT TITLE:	<i>KART BUS FACILITY</i>
DESCRIPTION:	<i>KART BUS FACILITY SOLAR & EV CHARGING IMPROVEMENTS</i>
APPLICANT:	<i>DUBNEY, ALEXANDER KIMLEY-HORN AND ASSOCIATES, INC ID-000027736</i>
LOCATION:	<i>629 W. DAVIS, HANFORD, CA, 93230</i>
APN:	<i>012161048000</i>

THE DEVELOPMENT SHALL COMPLY WITH ALL APPLICABLE CITY OF HANFORD STANDARDS AND SPECIFICATIONS, WITH ANY DEVIATIONS REQUIRING PRIOR APPROVAL FROM THE CITY ENGINEER. THE DEVELOPER IS SOLELY RESPONSIBLE FOR ALL FINANCIAL AND OTHER ARRANGEMENTS RELATED TO THE PLANNING, ENGINEERING, SCHEDULING, INSTALLATION, RELOCATION, OR UNDERGROUNDING OF PRIVATELY-OWNED UTILITIES, INCLUDING ELECTRIC, GAS, AND TELECOMMUNICATIONS. SIGNIFICANT PROJECT DELAYS MAY OCCUR IF UTILITY ISSUES ARE NOT RESOLVED BEFORE CONSTRUCTION BEGINS.

PROJECT COMMENTS/CONDITIONS

THE UTILITIES & ENGINEERING DEPARTMENT HAS REVIEWED THE SUBMITTED PLANS FOR THE ABOVE-REFERENCED PROJECT AND APPROVES THE PLANS SUBJECT TO THE CONDITIONS NOTED BELOW AND ON THE SUBMITTAL. ALL LISTED CONDITIONS SHALL BE ADDRESSED AND INCORPORATED INTO THE PROJECT.

ALL PUBLIC WORKS, UTILITIES, AND ENGINEERING CONDITIONS SHALL BE INCORPORATED INTO THE BUILDING PERMIT SUBMITTAL PLANS AND DOCUMENTS FOR REVIEW AND APPROVAL BY THE ENGINEERING DIVISION.

• ENCROACHMENT PERMIT

FOR ALL WORK CONDUCTED WITHIN THE CITY RIGHT-OF-WAY, A PROPERLY LICENSED CONTRACTOR MUST OBTAIN AN ENCROACHMENT PERMIT FROM THE UTILITIES & ENGINEERING DEPARTMENT PRIOR TO PERFORMING ANY WORK. APPLY FOR THE PERMIT THROUGH THE CITY'S CITIZEN SELF SERVICE PORTAL

• REFUSE SERVICE REQUIREMENTS

THE EXISTING TRASH ENCLOSURE SHALL BE EVALUATED AND MODIFIED AS NECESSARY TO FULLY COMPLY WITH CITY STANDARD GE-35. THIS INCLUDES, BUT IS NOT LIMITED TO, ENCLOSURE DIMENSIONS, ACCESSIBILITY, MATERIALS, AND SERVICE ACCESS REQUIREMENTS. IF THE EXISTING ENCLOSURE CANNOT BE BROUGHT INTO COMPLIANCE, IT SHALL BE REMOVED AND REPLACED WITH A NEW ENCLOSURE MEETING CURRENT CITY STANDARDS.

• DRIVE APPROACH REQUIREMENTS

THE DRIVEWAY APPROACH ON WILLIAMS STREET SHALL BE REMOVED AND RECONSTRUCTED, OR MODIFIED AS NECESSARY, TO FULLY COMPLY WITH CITY STANDARD CO-41. ALL DIMENSIONS, WIDTHS, AND GRADES SHALL BE DESIGNED AND SHOWN ON THE IMPROVEMENT PLANS AND SUBJECT TO REVIEW AND APPROVAL BY THE ENGINEERING DIVISION.

• GENERAL REQUIREMENTS

ANY EXISTING SIDEWALK, CURB, AND GUTTER ALONG THE PROJECT FRONTAGE THAT ARE DETERMINED TO BE IN DISREPAIR, NON-COMPLIANT, OR DAMAGED DURING CONSTRUCTION SHALL BE REMOVED AND REPLACED. ALL REPLACEMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH APPLICABLE CITY STANDARDS AND SHALL MEET CURRENT ADA REQUIREMENTS, AS VERIFIED BY THE PUBLIC WORKS INSPECTOR. LOCATIONS REQUIRING REPLACEMENT SHALL BE CLEARLY IDENTIFIED ON THE IMPROVEMENT PLANS.

PROJECT COMPLETION:

COMPLETION OF IMPROVEMENTS: ALL PROJECTS MUST FULLY IMPLEMENT EVERY IMPROVEMENT OUTLINED IN THE SIGNED AND APPROVED PLANS PRIOR TO OCCUPANCY AND ISSUANCE OF FINAL BUILDING PERMITS. NO OCCUPANCY OR FINAL PERMITS WILL BE GRANTED UNTIL ALL IMPROVEMENTS ARE VERIFIED AS COMPLETE PER THE APPROVED PLANS. EXCEPTIONS REQUIRE PRIOR WRITTEN APPROVAL FROM THE RELEVANT REGULATORY AUTHORITY.

FINAL DEFICIENCY (PUNCH) LIST REQUIREMENTS: PRIOR TO THE ISSUANCE OF A NOTICE OF COMPLETION, THE CONTRACTOR AND/OR DEVELOPER MUST REQUEST A COMPREHENSIVE FINAL DEFICIENCY (PUNCH) LIST INSPECTION TO VERIFY THAT ALL IMPROVEMENTS HAVE BEEN CONSTRUCTED IN ACCORDANCE WITH THE APPROVED PLANS. THE PUNCH LIST PROCESS INVOLVES A THOROUGH ON-SITE REVIEW BY AUTHORIZED INSPECTORS TO IDENTIFY ANY OUTSTANDING ITEMS, DEFICIENCIES, OR NON-COMPLIANT ELEMENTS.

- REQUESTING THE PUNCH LIST: UPON COMPLETION OF ALL IMPROVEMENTS, THE CONTRACTOR AND/OR DEVELOPER MUST REQUEST A "FINAL DEFICIENCY (PUNCH) LIST" FROM THE AUTHORIZED INSPECTOR.
- CORRECTION TIMELINE: ALL CORRECTIONS IDENTIFIED IN THE FINAL PUNCH LIST MUST BE COMPLETED WITHIN 30 DAYS OF ISSUANCE. IF CORRECTIONS ARE NOT COMPLETED WITHIN THIS PERIOD, THE PUNCH LIST BECOMES VOID, AND A NEW FINAL PUNCH LIST MUST BE REQUESTED. DURING RE-INSPECTIONS CONDUCTED AFTER THE 30-DAY TIMEFRAME, ANY ADDITIONAL DEFICIENCIES IDENTIFIED MAY BE ADDED TO THE UPDATED PUNCH LIST.
- RE-INSPECTION PROCESS: UPON COMPLETION OF ALL CORRECTIONS, THE CONTRACTOR AND/OR DEVELOPER MUST CONTACT THE INSPECTOR TO SCHEDULE A RE-INSPECTION TO VERIFY COMPLIANCE.
- COMPLETION REQUIREMENT: ALL ITEMS LISTED ON THE FINAL PUNCH LIST MUST BE FULLY ADDRESSED AND VERIFIED BY THE INSPECTOR BEFORE THE PROJECT IS ACCEPTED FOR FINAL COMPLETION.

SUBMISSION OF DIGITAL FILES: PRIOR TO THE ISSUANCE OF A NOTICE OF COMPLETION, THE CONTRACTOR AND/OR DEVELOPER MUST SUBMIT THE FOLLOWING TO THE CITY:

- A COMPLETE CAD FILE (.DWG FORMAT) CONTAINING ALL PROJECT DRAWINGS, DETAILS, AND SPECIFICATIONS.
- A SHAPEFILE (.SHP FORMAT) COMPATIBLE WITH GEOGRAPHIC INFORMATION SYSTEMS (GIS) FOR INTEGRATION INTO THE CITY'S SPATIAL DATABASE.

THESE FILES MUST ADHERE TO THE CITY'S SPECIFIED FORMATS AND INCLUDE ANY REQUIRED METADATA OR DOCUMENTATION TO ENSURE USABILITY. NON-COMPLIANCE WITH THESE REQUIREMENTS MAY RESULT IN DELAYS, DENIAL OF OCCUPANCY, WITHHOLDING OF PERMITS, OR OTHER ENFORCEMENT ACTIONS. THE CONTRACTOR AND/OR DEVELOPER ACKNOWLEDGES THAT FINAL PROJECT CLOSURE IS CONTINGENT UPON FULL COMPLIANCE WITH THESE TERMS.

Notice of Exemption 2026-30

To: Office of Planning and Research
P.O. Box 3044, Room 212
Sacramento, CA 95812-3044

County Clerk
County of Kings
Kings County Government Center
Hanford, CA 93230

From: City of Hanford
317 North Douty Street
Hanford, CA 93230

Project Title: Variance 0031-26

Project Location – 629 W. Davis St. (APN 012-161-048)

Project Location – City: Hanford Project Location – County: Kings County

Description of Nature, Purpose and Beneficiaries of Project: Variance No. 0031--26, a request to deviate from the standards set forth in Section 17.38.060 of the Hanford Municipal Code, in order to locate bus parking spaces, solar canopy structures, electrical infrastructure, and allow for bus maneuverability within the required front setback area for the expansion of an existing Kings Area Regional Transit (KART) bus facility in the PF Public Facilities zone district.

Name of Public Agency Approving Project: City of Hanford, Planning Division

Name of Person or Agency Carrying Out Project: Kimley-Horn and Associates, INC
Address: 401 B St. Suit 600 Unit: Suit 600 San Diego, CA 92101
Phone Number: (619)452-2200

Exempt Status: **(check one)**

- Ministerial (Sec. 21080(b)(1); 15268);
- Declared Emergency (Sec. 21080(b)(3); 15269 (a));
- Emergency Project (Sec. 21080(b)(4); 15269(b)(c));
- Categorical Exemption: State type and section number: 15303 New Construction or Conversion of Small Structures.
- Statutory Exemption. State code number:

Reasons why project is exempt:

The City of Hanford determined the project to be exempt from environmental review pursuant to Section 15303 of the California Environmental Quality Act (CEQA) Guidelines. Section 15303 states that Class 3 exemptions consist of construction and location of limited numbers of new, small facilities or structures; installation of small new equipment and facilities in small structures; and the conversion of existing small structures from one use to another where only minor modifications are made in the exterior of the structure. This project involves the installation of accessory solar canopies and service equipment.

Lead Agency

Contact Person: Maddison Faria Area Code/ Telephone: (559) 415-7183

Signature: _____ Date: May 26, 2026 Title: Assistant Planner

- Signed by Lead Agency Date received for filing at OPR: _____
- Signed by Applicant